



PLANNING PROPOSAL

34-42 East Street, Granville

Planning Proposal drafts

Proponent versions:

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1.	ThinkPlanners	June 2017

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INTRODUCTION

This planning proposal seeks to increase the current permitted maximum height of building control under the Parramatta Local Environmental Plan 2011 and retain the existing zoning and density controls at 34-42 East Street, Granville. The intention of the Planning Proposal is to broadly apply the recommendations of the Parramatta Road Corridor Urban Transformation Strategy (PRCUTS) to address the current 'mismatch' of the existing height and FSR controls permitted on the B4 Mixed Use land in Granville by increasing the maximum Height of Building control. This mismatch has become apparent through the assessment of recent Development Applications within the B4 Mixed Use zone in Granville.

This planning proposal has been prepared in accordance with Section 55 of the *Environmental Planning and Assessment Act 1979* and the Department of Planning and Environment's, 'A Guide to Preparing Local Environment Plans' (April 2013) and 'A Guide to Preparing Planning Proposals' (October 2012).

Background and context

On 27 April 2017 the applicant DPG Project Pty Ltd on behalf of the landowner St Vincent De Paul Society NSW lodged a Planning Proposal with the City of Parramatta for the land at 34-42 East Street, Granville.

The subject site is 34-42 East Street, Granville. It has an approximate site area of 1,577m², and is made up of three allotments:

- Lot 1 DP 1009146
- Lot 1 DP 195784
- Lot 1 DP 996285.

The site is bound by East Street to the north and the Western Railway Line to the south as seen in **Figure 1**.



Figure 1 – Locational Map

The subject site is occupied by a range of land uses including business, light industrial, and low density residential. The land surrounding the subject site currently comprise a mixture of industrial uses and some single to three storey residential developments.

The Granville Precinct is experiencing renewal. A number of development applications have been lodged with Council for the redevelopment of the existing industrial uses and low density residential uses to mixed use developments that have a large residential component. **Figure 2** identifies the subject site and the recently approved development applications in immediate proximity to the subject site.

It is noted that the site subject to the Planning Proposal was the subject of a recent development approval that formed part of a larger development site that was approved for a mixed use development as part of DA/738/2014 (DA Site No.3 in **Figure 2**). This DA was one of the DAs within the precinct that was unable to achieve the permitted FSR of 6:1 under the PLEP 2011 due to the height control (even with a Clause 4.6 variation). Nonetheless, the subject land currently has an active development consent for its redevelopment which could proceed irrespective of the planning proposal progressing.

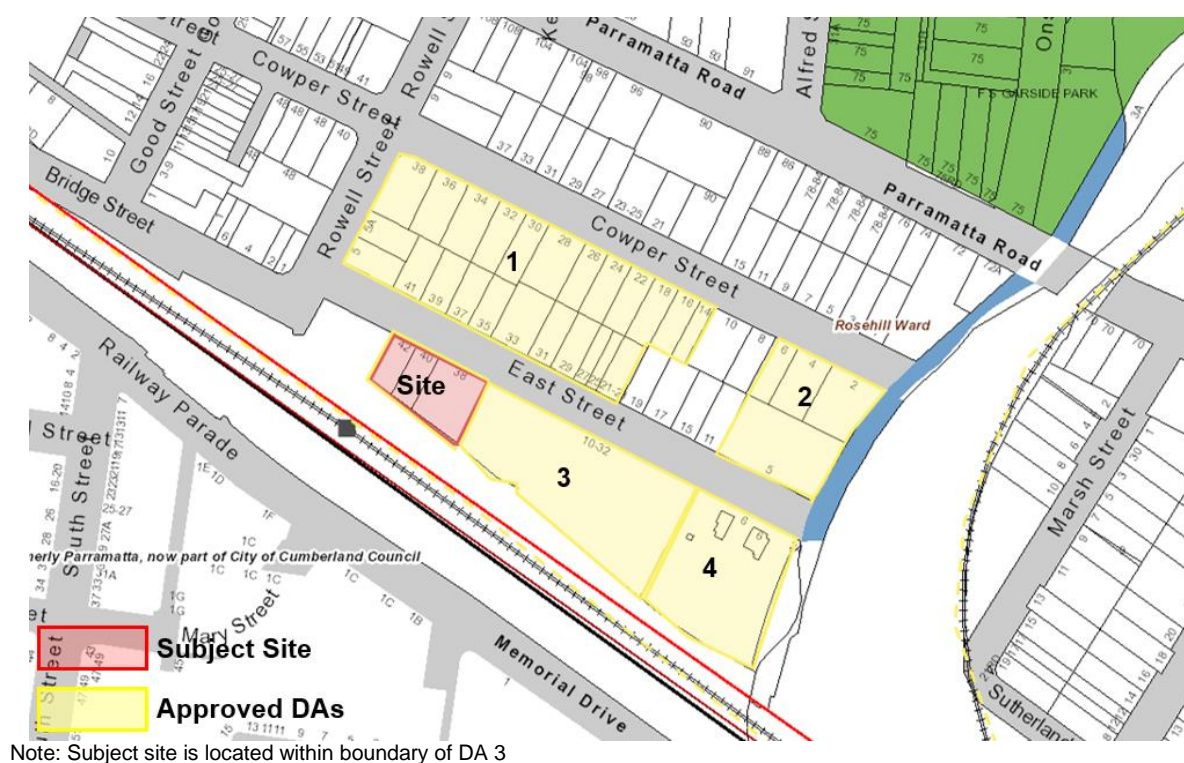


Figure 2 – Approved DAs in proximity to subject site

Table 1 summarises the details of the approved developments shown in **Figure 2**, which are labelled 1, 2, 3 and 4. As demonstrated within the table, the applications were not able to attain the permitted FSR under the permitted height controls.

	Address	Development	Permitted FSR	Approved FSR	Permitted Height	Approved Height
1	14-38 Cowper Street, 21-41 East Street & 5-5A Rowell Street	Deferred commencement approval for the construction of a mixed use development consisting of 618 residential apartments, 12 commercial tenancies, and 633 car parking spaces within a building with a 4 storey podium and 3 tower forms with varying heights from 14 to 21 storeys	6:1	5.5:1 -8% variation	52m	70m Council permitted a 35% variation through Clause 4.6
2	2-6 Cowper Street & 1-9 East Street	Deferred commencement approval for 20 storey mixed use development comprising 5 retail tenancies and 264 residential units, 255 car parking spaces, and landscaping and construction of a public vehicular lane.	6:1	6:1	52m	68m Council permitted a 31% variation through Clause 4.6
3	10-42 East Street (which includes the subject site)	Approval for a mixed use development comprising 463 residential dwellings and 6 commercial tenancies across three buildings: Block A, B and C. Block A and B have already commenced construction	6:1	5.5:1 -8% variation	52m	61m Council permitted a 17% variation through Clause 4.6
4	2-8 East Street	Approval for a 19 storey mixed used development containing 211 sqm of commercial floor space and 208 apartments over 4 levels of basement carparking.	6:1	5.18:1 -14% variation	52m	64m Council permitted a 23% variation through Clause 4.6

Table 1 – Existing DA Approvals

In summary, **Table 1** demonstrates that the existing permitted FSR of 6:1 cannot be delivered under the existing permitted height of 52m and that there is a mismatch between the built form controls. The previous approvals have relied on a Clause 4.6 variation to the permitted height to achieve an FSR that is close to 6:1, with many still not achieving an FSR of 6:1 even with the variation under Clause 4.6. The resulting urban design outcome has been suboptimal within the approved developments, with the restricted height resulting in relatively squat fatter buildings all of a similar height.

This development experience in Granville has highlighted a flaw in the planning controls, and serves as the catalyst of this Planning Proposal. The sought controls within this proposal aim to provide a height control that will enable to attainment of the existing FSR of 6:1.

PART 1 – OBJECTIVES OR INTENDED OUTCOMES

The objective of this planning proposal is to enable the subject site to achieve the existing permitted FSR of 6:1 by increasing the current permitted maximum height of building control from 52m to 82m under the Parramatta Local Environmental Plan 2011. The intention of the Planning Proposal is to broadly apply the recommendations of the Parramatta Road Corridor Urban Transformation Strategy (PRCUTS) to address the current ‘mismatch’ of the existing height and FSR controls experienced on the B4 Mixed Use zoned land in Granville. This mismatch and constraint prevented the site achieving the permitted FSR as part of a previously lodged and assessed DA. It is anticipated that the Planning Proposal will provide controls that enable a built form that achieves an FSR of 6:1. It will also assist in the delivery of the dwelling targets under the PRCUTS, and comply with the strategic planning framework for Granville. The change to the planning controls will likely result in the lodgement of a DA for a mixed use development with a large residential component, located within a podium and tower.

PART 2 – EXPLANATION OF PROVISIONS

This planning proposal seeks to amend *Parramatta Local Environmental Plan 2011 (PLEP 2011)* in relation to the height and FSR controls. Whilst the Planning Proposal does not seek to increase the existing permitted FSR, changes to the FSR map are required to remove the application of the existing sliding scale that prescribes the eligible FSR for the subject site. Therefore the Planning Proposal seeks to ‘fine tune’ the existing FSR controls.

In order to achieve the desired objective, the following amendments to *PLEP 2011* would need to be made:

1. Increase the Maximum Height of Building control from 52m to 82m, and remove the site’s inclusion from ‘Area 1’ on the Height of Building Map to remove the application of the sliding scale provision under Clause 4.3(2A) of the PLEP 2011.
2. Retain the existing 6:1 Maximum Floor Space Ratio control, however remove the site’s inclusion from ‘Area 1’ on the FSR map to remove the application of the sliding scale provisions within Clause 4.4(2A) of the PLEP 2011.
3. Introduce a Site Specific Clause to exclude wintergardens (enclosed balconies) in the south-eastern corner of the site from the calculation of the future development’s gross floor area calculation to manage amenity constraints from the railway line.

2.1 Other relevant matters

2.1.1 Site Specific Development Control Plan

A Site Specific DCP is recommended to be prepared for the subject site to control the built form configuration and apportionment of FSR, secure building setbacks, design the public domain and access arrangements, apply the Parramatta Road Corridor Urban Transformation Strategy car parking rates, and introduce the alternative design excellence process. The preparation of the DCP will also address the urban design matters that require further investigation and refinement to ensure an appropriate built form and public domain is delivered on the subject site. The Site Specific DCP will be exhibited with the Planning Proposal should a Gateway Determination be issued.

PART 3 – JUSTIFICATION

This part describes the reasons for the proposed outcomes and development standards in the planning proposal.

3.1 Section A - Need for the planning proposal

This section establishes the need for a planning proposal in achieving the key outcome and objectives. The set questions address the strategic origins of the proposal and whether amending the LEP is the best mechanism to achieve the aims of the proposal.

3.1.1 Is the Planning Proposal a result of any study or report?

The planning proposal is a result of an application from the landowner seeking to increase the maximum height of building control on the site. The planning proposal broadly seeks to implement the recommendations of the Parramatta Road Corridor Urban Transformation Strategy (PRCUTS) and address the mismatch in the existing height and FSR controls experienced within Granville under the PLEP 2011.

The PRCUTS is an integrated land use and transport plan to revitalise Parramatta Road by delivering housing, employment, public transport, open space and amenity needs. The PRCUTS projects the delivery of 27,000 new homes and 50,000 new jobs along the Parramatta Road Corridor. The renewal will be focused in eight strategic Precincts, one of which is Granville. The recommendations of the PRCUTS are to be implemented by the planning proposal process, with the strategy being given statutory weight via a Ministerial Direction, under Section 117 of the *Environmental Planning and Assessment Act 1979*.

Therefore, the Planning Proposal has been lodged in response to the PRCUTS and to implement its recommendations via the S117 Direction. Furthermore, the Planning Proposal is the result of a previous development application assessment and approval process which demonstrated an inconsistency between the height and FSR controls that apply to their site. With this serving as the catalyst for the lodgement of the Planning Proposal, the proposal also seeks to apply the recommendations of the adopted strategic framework for the Granville Precinct.

3.1.2 Is the Planning Proposal the best means of achieving the objectives or intended outcomes, or is there a better way?

A planning proposal seeking to amend *PLEP 2011* is the most effective way of providing certainty for Council, the local community and the landowner. The current height control does not allow the site to be developed to the existing density of 6:1, and a change to the LEP through a Planning Proposal is required to achieve the intended outcome of a development with an FSR of 6:1. Given the PRCUTS is to be implemented via a S177 Direction, a planning proposal is needed.

3.2 Section B – Relationship to strategic planning framework

This section assesses the relevance of the Planning Proposal to the directions outlined in key strategic planning policy documents. Questions in this section consider state and local government plans including the NSW Government's Plan for Growing Sydney and subregional strategy, State Environmental Planning Policies, local strategic and community plans and applicable Ministerial Directions.

3.2.1 Is the planning proposal consistent with the objectives and actions contained within the applicable regional or sub-regional strategy?

The stated objective of the planning proposal closely aligns with several State Government strategies, specifically A Plan for Growing Sydney, the draft amendments to A Plan for Growing Sydney, the draft West Central District Plan and the Parramatta Road Corridor Urban Transformation Strategy.

A Plan for Growing Sydney

On 14 December 2014, the NSW Government released 'A Plan for Growing Sydney' which outlines actions to achieve the Government's vision for Sydney which is a 'strong global city and a great place to live'.

Parramatta local government area is part of the West Central Subregion. A Plan for Growing Sydney identifies the following directions, actions and priorities for Parramatta and the West Central Subregion that are relevant to this site and planning proposal:

1.7 Grow Strategic Centres – providing more jobs closer to home

- Invest in strategic centres across Sydney to grow jobs and housing and create vibrant hubs of activity

2.1 Accelerate housing supply across Sydney

- Accelerate housing supply and local housing choices
- Accelerate new housing in designated infill areas (established urban areas) through the priority precincts and UrbanGrowth NSW programs

2.2 Accelerate urban renewal across Sydney – providing homes closer to jobs

- Use the Greater Sydney Commission to support Council-led urban infill projects
- Undertake urban renewal in transport corridors which are being transformed by investment, and around strategic centres

2.3 Improve housing choice to suit different needs and lifestyles

- Require local housing strategies to plan for a range of housing types

West Central Subregion

- Accelerate housing supply, choice and affordability and build great places to live
- Provide capacity for additional mixed use development in Parramatta CBD and surrounding precincts including offices and retail in Parramatta CBD, arts and culture in Parramatta and housing in all precincts

A Plan for Growing Sydney outlines the State Government's plan to deliver homes, jobs and infrastructure to support a growing population. Key directions described in the Plan relate to accelerating housing supply and urban renewal across Sydney, with a focus on providing homes in areas well serviced by existing or planned infrastructure.

The Plan identifies the gap between current housing production and future housing needs and that it is critical to remove the barriers to increased housing production to facilitate accelerated housing supply. The Plan explains that the Government intends to work with councils and the development sector to put in place flexible planning controls which enable housing development in feasible locations.

The Plan states that the Government intends to focus on the urban renewal of the Parramatta Road corridor, within which the subject site is situated. As noted in the Plan, the Parramatta Road corridor has good access to employment, a rail network and a range of social infrastructure.

The planning proposal will enable the development of residential dwellings and non-residential uses that will contribute towards dwelling and employment targets on a site located within the Granville Precinct. Approximately 119 units with a range of unit types are proposed to promote housing supply and choice. The non-residential uses on the site support Council's vision to provide retail and commercial uses in the Granville Centre to support the population to result from the anticipated growth envisaged under the PRCUTS.

The planning proposal seeks to relax building height and floor space ratio controls to allow the delivery of high-density housing in an area that can accommodate increased population. Granville is serviced by public transport services that are frequent and is located close to the Parramatta CBD. By allowing development of taller and more slender buildings in this location there are significant public benefits including the provision of additional green grid and ground level street activation with a highly integrated public domain.

The planning proposal is consistent with the objectives and actions of A Plan for Growing Sydney

Towards Our Greater Sydney 2056

In November 2016 the Greater Sydney Commission released Towards Our Greater Sydney 2056 (TOGS), a draft amendment to A Plan for Growing Sydney, for public exhibition. TOGS aligns with the district plans, released on the same date. Both TOGS and the district plans were on exhibition until March 2017.

The vision for the future of Sydney as identified in TOGS is for decentralisation with centres supported by public transport. TOGS introduces the concept of three cities; the Eastern City; the Central City; and the Western City. Parramatta LGA (which is inclusive of Granville) is located in the Central City and identified as the anticipated greatest area of growth over the next decade. This planning proposal is in keeping with the vision of TOGS by providing employment and housing opportunities in close proximity to public transport in the Central City zone.

TOGS identifies the integration of the economy, social needs and the environment to move Sydney forward. The district plans provide further detail as to how a productive Sydney, a liveable Sydney and a sustainable Sydney will be achieved.

The TOGS presents a polycentric approach to Sydney which reinforces the significant role of Greater Parramatta as the Central City within Greater Sydney. TOGS revises population growth levels, which have increased since the release of A Plan for Growing Sydney. To accommodate this population growth and respond to issues of housing affordability housing supply in key locations including Parramatta and Granville need to be accelerated.

Urban renewal is a key priority particularly with the focus of new housing in existing centres with frequent public transport that can carry large number of passengers and in locations that have already the community infrastructure in place such as medical services, social services, educational facilities, recreational opportunities and employment. The revitalisation to result from this Planning Proposal within a well established centre such as

Granville will deliver the accelerated housing, provide more affordable housing typologies and a greater variety of housing choice.

Greater Parramatta and the Olympic Peninsula Plan (Greater Sydney Commission October 2016)

The GPOP has been identified as a new priority growth area in A Plan for Growing Sydney 2014. The GPOP sets forward a vision to build upon four quarters identified within the area. Each quarter of the GPOP is linked by the Parramatta River and the planned Parramatta Light Rail.

Granville, although not discussed within the GPOP and its renewal/growth is covered by the Parramatta Road Corridor Urban Transformation Strategy, is shown as fitting within the first quarter of the Parramatta CBD and Westmead Health and Education Super Precinct. Again, within this Plan the 30-minute city approach is adopted to ensure that access to all modes of transport connections are optimised. The GPOP advocates a break in the traditional west to east movement to make Parramatta CBD the central city within Greater Sydney.

The planning proposal is aligned with the GPOP in that it seeks to deliver a mix of housing, enabling residents of all kinds to settle and move as their needs change from student to entrepreneur, medico or executive. The planning proposal is also closely aligned with the desire to deliver new dwellings within the 30-minute city model with the proposed new dwellings within Granville being within 2km of Parramatta CBD.

Draft West Central District Plan

The draft West Central District Plan sets out the priorities and actions for this District and these are structured around 3 key themes of a Productive City, a Liveable City and a Sustainable City. As relevant to the subject site the importance of Parramatta's CBD and growing this CBD in terms of both jobs and housing are continually emphasised in the draft District Plan (particularly with reference to the 30-minute city). The planning proposal seeks to deliver both additional housing but also jobs within a 30-minute city scenario.

Delivering more dwellings and within shorter timeframes aims to respond to the current housing demand. The planning proposal seeks to deliver housing to the market quickly and in location which is well within the 30-minute city scenario and within walking/cycling distance of the Parramatta CBD.

In summary, this planning proposal seeks to deliver on the vision set forward in the draft West Central District Plan by:

- Increasing diversity of housing choice
- Delivering housing to meet both the 5 and 20-year strategic housing supply targets. Parramatta is scheduled to deliver more than 21,000 new homes over the coming 5 years which is ambitious target that can only be met with significant increases to permitted building heights and FSRs. The Parramatta Road Corridor Urban Transformation Strategy reflects this demand by recommending an increasing in density for Granville.
- Expediting the delivery of new housing stock to ease the pressure of demand resulting in a generally more affordable housing product.
- Contributing to energy efficiency through aims to deliver a development that meets environmental performance criteria

- Reducing emissions through both building environmental performance but also through reduction in reliance on private vehicle travel. Focusing increased housing on the subject site which is highly accessible to local bus and train services means that future residents are more likely to walk, cycle and use integrated public transport systems.
- Enhancing the role of Greater Parramatta as the economic anchor within the Greater Parramatta Olympic Peninsula vision by delivering both jobs and housing.
- The objectives of the planning proposal are considered to align closely with the documented priorities for the West Central District.

Parramatta Road Corridor Urban Transformation Strategy

The Parramatta Road Corridor Urban Transformation Strategy (PRCUTS) prepared by UrbanGrowth NSW was released by the Minister for Planning on 9 November 2016. The Strategy's key objective as an integrated land use and transport plan is to revitalise Parramatta Road, by delivering future housing, employment, public transport, open space and amenity needs.

The PRCUTS projects the delivery of 27,000 new homes and 50,000 new jobs along the Parramatta Road Corridor. The renewal will be focused in eight strategic Precincts at Granville, Auburn, Homebush, Burwood, Kings Bay (part of Five Dock), Taverners Hill, Leichhardt, and Camperdown.

Specifically, for the Granville Precinct the PRCUTS projects the following growth:

Granville Precinct	Population	Dwellings	Jobs
PRCUTS	10,700	5,400	7,200

Table 2 - Projected population, dwellings and jobs for the Granville Precinct

Recommend Planning Controls in the PRCUTS

The PRCUTS (November 2016) provides recommended zoning, floor space ratio, and height controls for each site within the Granville Precinct. To achieve the targets above, the recommended zoning, height and FSR controls for the precinct are shown in **Figure 3, 4 and 5**.

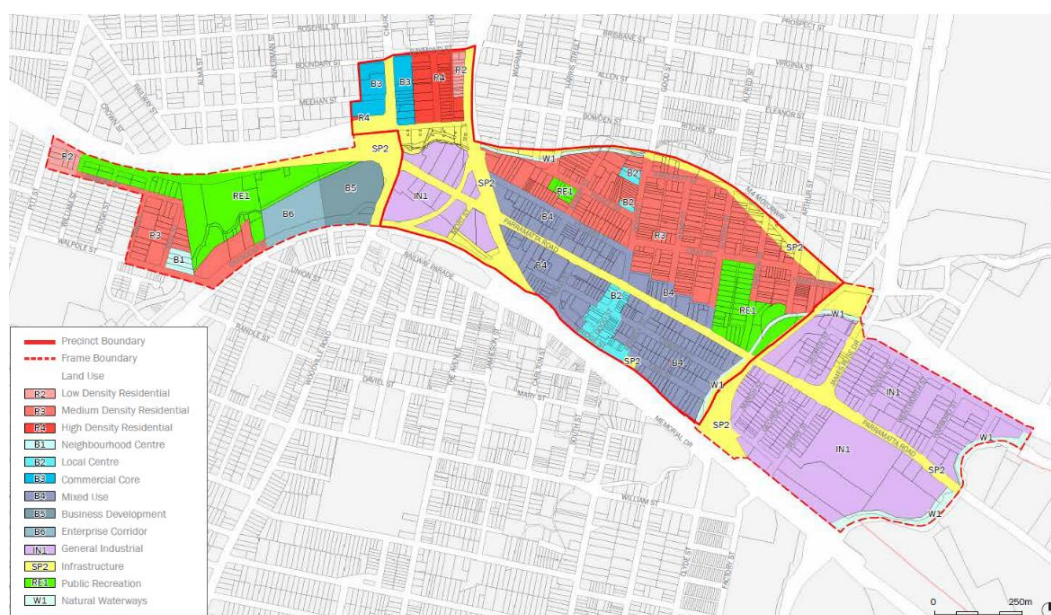


Figure 3 – Recommended zoning from PRCUTS

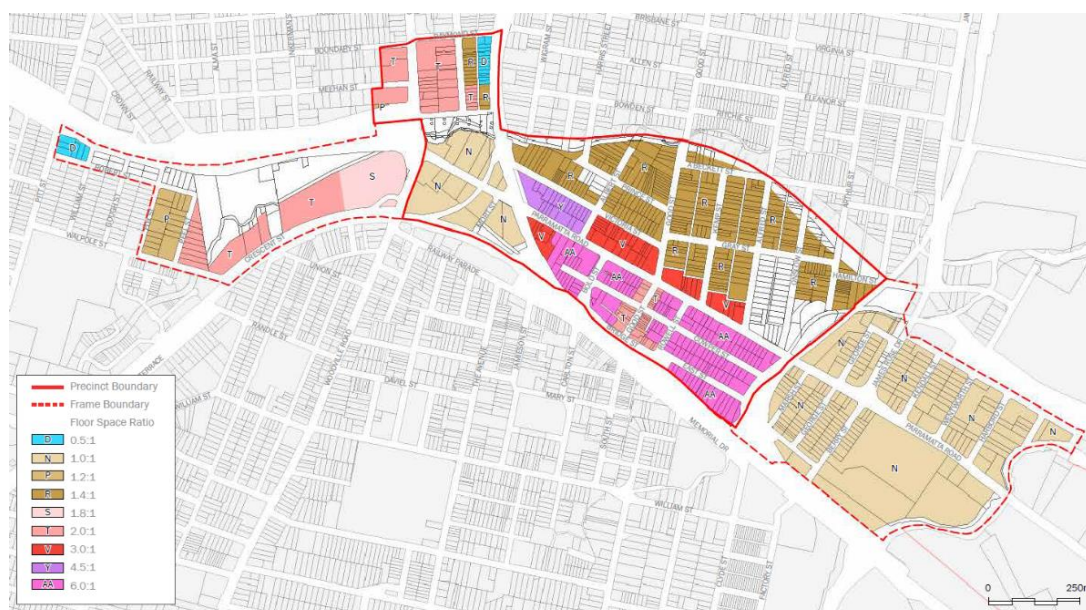


Figure 4 – Recommended Floor Space Ratios from PRCUTS

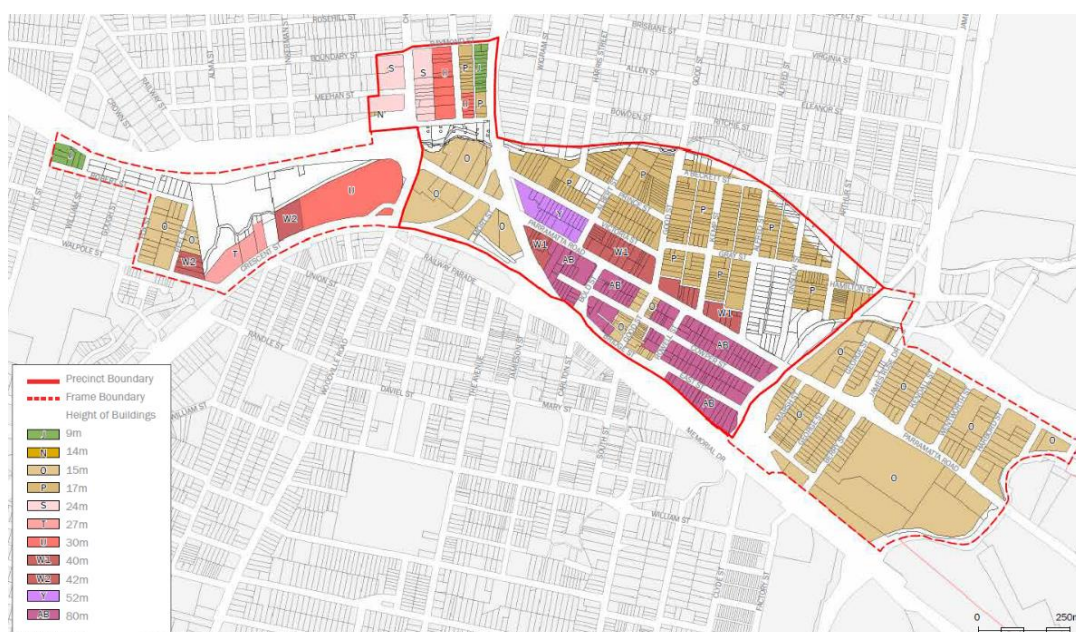


Figure 5 – Recommended Height of Buildings from PRCUTS

One of the strategic initiatives to address the housing target under the PRCUTS is to:

- Extend the existing B4 Mixed Use zone to the majority of the precinct across both sides of Parramatta Road (as seen in **Figure 3**)
- Apply an FSR of 6:1 (subject to a sliding scale) similar to the way density is currently managed under the PLEP 2011
- Apply a height of 80m (approximately 25 storeys) to address the current mismatch between the 52m height and 6:1 FSR controls within the PLEP 2011.

A summary of the sought changes to the PLEP 2011 under the Planning Proposal and the recommendations of the PRCUTS are summarised in **Table 3**:

	PRCUTS	Planning Proposal
Zoning	B4 Mixed Use	B4 Mixed Use
Height	80m subject to sliding scale	82m no sliding scale
FSR	6:1 subject to sliding scale	6:1 no sliding scale

Table 3 – Comparison of PRCUTS and the Planning Proposal

The Planning Proposal is broadly consistent with the intention of the PRCUTS and seeks to increase the maximum permitted height to enable the delivery of the existing FSR 6:1 in the existing B4 Mixed Use zone. Whilst there is a minimum inconsistency with the height as seen in Table 3, this is considered minor and the Urban Design Report and supporting reference design has demonstrated that the proposal will result in a better built form outcome than what is currently approved on the subject site. This is discussed in further detail below. Overall the Planning Proposal is consistent with the strategic framework of the PRCUTS and aims to revitalise the subject site, contribute to dwelling targets by providing approximately 119 dwellings, and provide a suite of controls that are matched and enable achievement of an FSR of 6:1.

The PRCUTS identifies seven strategies for transformation of the Corridor and after reviewing these principles the planning proposal is aligned with all relevant principles namely:

Housing choice and affordability

- The planning proposal seeks to expedite delivery of new housing to the market bringing forward supply to meet the growing demand.
- The typical floor plans provided with this planning proposal consider a range of compact and efficient housing designs that are seeking to provide flexible apartment designs to meet the needs of a range of future residents.

Diverse and resilient economy

- The proposal seeks to deliver increased density in a location that is already well-served by public transport, local bus services and the arterial road network.
- The mixed-use zone encourages ground floor commercial uses, which have been incorporated into this proposal. Co-location of multiple uses and offering flexibility means Granville can grow as needed to suit the needs of the future residents making this a diverse and resilient community and economy.
- Commercial spaces at the ground floor are purposely designed to allow flexibility in future uses. These spaces may in future be occupied as larger premises or further broken down into smaller commercial suites.

Accessible and connected

- Promotion of sustainable transport choices is one of the key drivers to delivery of increased building height and density on this site.
- The site is closely connected to the Duck Creek Corridor, and potential future cycle paths. There are also good connections available through to the north and further onto the Parramatta CBD. This close connection to alternative travel choices will result in increased trips on foot or by bicycle.

Vibrant communities and places

- The planning proposal seeks to deliver a 15-minute neighbourhood through delivering housing with improved walkability, cycling and safety that will support healthier communities.
- The proposed increased density and building height on this site is very much aligned with the Strategy to improve walkability, housing choice, provision of useable/safe open spaces, commercial space for local services and infrastructure all within an exceedingly accessible location.

Green spaces and links

- The proposed options for redevelopment show the site is capable of delivering a valuable contribution to the green grid with public domain works and communal open space.
- 100% of proposed dwellings within the development site will be conveniently located within 100m safe walking distance of high quality open space and public domain areas.
- Building setbacks as prescribed by the Strategy that aim to achieve new green setbacks are accommodated within the building envelopes provided with this planning proposal.

Sustainability and resilience

- At the detailed design stage further details relating to meeting water and energy reduction targets will be explored including a 10-15% car share take-up rate with the aim of reducing car use by 30%.

Design Excellence and Urban Design

The PRCUTS requires a Design Excellence process to be run for “sites with an inherent scale impact (greater than 1,500m² or proposals that exceed four storeys in height)”. The Planning Proposal is greater than 1500m² and therefore it is required to demonstrate design excellence.

The PRCUTS does not specify one specific mechanism for the delivery of design excellence. However it does pose the following requirements:

- Design excellence needs to be clear, transparent, provide certainty, and timely. Mechanisms to deliver design excellence might include:
 - independent and expert design review and panels
 - competitive selection processes
 - accountability and monitoring
 - clear relationships to other entities including adjacent councils regarding their panel selections, shared panellists, or specialist panels.

It is on record that Council deem that a design excellence competition process is the most appropriate mechanism to achieve design excellence. However, due to the circumstances of this proposal, and the existing approved design and consent, it was conceded that as part of the approval history of this site the relevant applicant has already gone through one design excellence process (i.e. Design Excellence Advisory Panel (DEAP)). The DEAP review because of the inconsistency between the height and FSR controls involved various iterations and reviews of the design. It is acknowledged where the DEAP process involves significant redesigns there is added cost to the applicant. In this case the cost was as a result of a problem with the controls, not necessarily any poor design process by the applicant or their consultants.

Given these circumstance an alternative process to the design competition was considered. It is proposed that the applicant prepare a design by a registered architect that goes through the Design Excellence Advisory Panel, and should the panel not consider the scheme to exhibit design excellence then the applicant will be required to go through the Design Excellence Competition process. Such a requirement is recommended to be embedded within a Site Specific DCP, which also has a specific criteria that the panel must consider when assessing the building design for the subject site. The criteria will present specific considerations relating to the public domain requirements and built form controls. This is to ensure a superior built form outcome is achieved on the subject site. This is considered in keeping with the intention of the PRCUTS and consistent with Council's desire to permit design excellence on new development within Granville.

A Site Specific DCP will embed specific Urban Design Controls in response to the PRCUTS and further ensure a good built form outcome is achieve, in line with the intention and recommendation of the PRCUTS.

Traffic Management

The Planning Proposal seeks to apply the car parking and bicycle parking rates specified in the PRCUTS. The PRCUTS provide maximum parking rates compared to the minimum current parking rates within the Parramatta DCP 2011. Council's Traffic Management Team are satisfied with the proposal providing parking at the rates specified within the PRCUTS (**Table 4**).

Table 3.2 Maximum Car Parking Rates

CATEGORY	RESIDENTIAL (MAXIMUM SPACES PER DWELLING)					OTHER (MAXIMUM SPACES/M ² GFA)		
	Studio	1 bed	2 bed	3 bed	Visitor	Commercial	Retail	Industrial
Camperdown Precinct and Frame Area Leichhardt Precinct and Frame Area Taverners Hill Precinct and Frame Area	0	0.3	0.7	1	0	150	100	150
Kings Bay Precinct and Frame Area Burwood Precinct and Frame Area Homebush Precinct only Granville Precinct Only	0.3	0.5	0.9	1.2	0.1	100	70	120
Auburn Precinct Homebush Frame Area Granville Frame Area	0.6	0.9	1.2	1.5	0.2	70	50	100

Table 4 – Maximum Parking Rates from PRCUTS

3.2.2 Is the planning proposal consistent with the local council's Community Strategic Plan or other local strategic plan?

The following strategic planning documents are relevant to the planning proposal.

Parramatta 2038 Community Strategic Plan

Parramatta 2038 is the Community Strategic Plan for the City of Parramatta. Parramatta 2038 is described as a long-term vision for the Parramatta Local Government Area and links to the long-term future of Sydney. The Plan describes Granville as a suburb that may experience a boost in land values with the delivery of improved quality transport links and attractive public spaces and identifies Granville as a suburb that will undergo urban renewal.

Parramatta 2038 identifies six strategic objectives to deliver the vision for Parramatta and the intended outcome of the planning proposal is considered to be consistent with these

objectives. Specifically, the redevelopment of the subject site will contribute to the economic growth of Parramatta, will encourage diversity and liveability of places and will enhance the status of Parramatta as a City in which people want to reside.

3.2.3 Is the planning proposal consistent with the applicable State Environmental Planning Policies?

The following State Environmental Planning Policies (SEPPs) are of relevance to the site (refer to Table 1 below).

Table 1 – Comparison of planning proposals with relevant SEPPs

State Environmental Planning Policies (SEPPs)	Consistent: Yes - ✓ No - ✗ or N/A	Comment
SEPP No 1 Development Standards	N/A	This SEPP is not applicable to the subject land under Clause 1.9 of the Parramatta LEP 2011.
SEPP 4 – Development Without Consent and Miscellaneous Exempt and Complying Development	N/A	This SEPP is not applicable to the subject land under Clause 1.9 of the Parramatta LEP 2011.
SEPP (State and Regional Development) 2011	N/A	Should the planning proposal proceed, it is likely that future development of the site will constitute Regional Development and be determined by the West Central Sydney Planning Panel.
SEPP 6 – Number of Storeys in a Building	N/A	Standard instrument definitions apply.
SEPP No 55 Remediation of Land	✓	<p>The Planning Proposal does not seek to rezone the subject site. The existing B4 Mixed Use is recommended to be retained, which already permits residential uses. As the Planning Proposal does not propose a zoning change, the requirements of SEPP 55 are not triggered.</p> <p>Nonetheless, the applicant prepared a preliminary environmental site investigation which reveals that the site has been used for various commercial/industrial land uses from the 1950s. The previous uses include mechanical repairs, potential car wash, warehouse and storage facilities, administration and training centres. The preliminary site investigation reveals that the site is considered to present a low to moderate risk of soil and groundwater contamination with the key area of concern for this site relating to the potential impact of soil and groundwater from the previous commercial/industrial land uses, the adjacent railway corridor and the presence of fill (which may contain former building demolition rubble).</p> <p>Based on the preliminary site investigation, additional investigations are recommended and will be required at the Development Application stage where the redevelopment of the site will need to address the requirements of the SEPP. The advice from the contamination report confirms that the site can be made suitable for the proposed development subject to the implementation of the recommendations in that report. Furthermore, the previous DA consent demonstrates the site is suitable and has already been subject to an assessment.</p>

		<p>Whilst not pertinent at the Planning Proposal stage the attached report will be required for the future DA.</p> <p>Appendix 2 includes the preliminary contamination report that accompanied the previous DA.</p>
SEPP 60 – Exempt and Complying Development	N/A	This SEPP is not applicable to the subject land under Clause 1.9 of the Parramatta LEP 2011.
SEPP 64 – Advertising and Signage	N/A	Not relevant to proposed amendment. May be relevant to future DAs.
SEPP No 65 Design Quality of Residential Flat Development	✓	<p>The planning proposal seeks to facilitate high density housing in the form of a residential flat building. An Urban Design Report is submitted with this planning proposal which considers a potential design option which address the provisions of SEPP 65. Council Officers have reviewed the Urban Design Report and the concept reference design included in the report and are satisfied that the requirements of the Apartment Design Guidelines under the SEPP can be met. Setbacks have been introduced to ensure the building separation and privacy requirements are met. Furthermore, whilst the land to the west of the site is currently used for special infrastructure uses by Sydney Trains, the concept design 'future proofs' the site by providing a 9m setback to ensure any future residential development on the site (should it turn over for residential uses) provide appropriate building separation. A further and more detail assessment of the future development's compliance with SEPP 65 will be carried out as part of the Design Excellence Advisory Panel process and future DA.</p>
SEPP No.70 Affordable Housing (Revised Schemes)	N/A	Not relevant to proposed amendment.
SEPP (Affordable Rental Housing) 2009	N/A	The planning proposal does not seek to introduce or provide for affordable housing. However future development may incorporate housing delivered under this SEPP and relevant provisions will be given detailed consideration during the assessment of a development application.
SEPP (BASIX) 2004	N/A	Detailed compliance with SEPP (BASIX) will be demonstrated at the time of making a development application for the site facilitated by this Planning Proposal.
SEPP (Exempt and Complying Development Codes) 2008	✓	This SEPP is not relevant in the context of the planning proposal. May apply to future development of the site.
SEPP (Infrastructure) 2007	✓	<p>Future development may constitute traffic generating development and trigger an assessment under this SEPP.</p> <p>The Planning Stage Acoustic Assessment submitted with this planning proposal also addresses the criteria required by clause 102 of this SEPP in terms of the impact of road noise or vibration on non-road development.</p>

Sydney Regional Environmental Plan No 18–Public Transport Corridors	N/A	This SEPP is not applicable to the subject land under Clause 1.9 of the Parramatta LEP 2011.
Sydney Regional Environmental Plan (Sydney Harbour Catchment) 2005	N/A	The proposed development is not located directly on the Sydney Harbour Catchment foreshore. Any potential impacts as a result of development on the site, such as stormwater runoff, will be considered and addressed appropriately at DA stage.
SEPP (Urban Renewal) 2010	✓	<p>The subject site is within the Granville Urban Renewal Precinct. The planning proposal is consistent with the aims and provisions of this SEPP. The proposed consolidation of lots within the subject site to facilitate high-density residential development aligns with the intent of this SEPP. The site forms part of the Parramatta Road Corridor Urban Transformation Strategy as referenced previously and Granville being 1 of 8 strategic precincts within this study. Granville has been the subject of extensive studies and investigations and continually put forward as an ideal location for high density development. The redevelopment of this site provides a significant renewal opportunity.</p> <p>The planning proposal is consistent with the SEPP given its land use and built form outcomes reflect those outlined by the various applicable strategies, as well as the Parramatta Road Corridor Urban Transformation Strategy and WestConnex.</p>
SEPP No. 32 - Urban Consolidation (Redevelopment of Urban Land)	✓	The planning proposal is consistent with the aims and provisions of this SEPP. The intent of the planning proposal seeks to increase housing density in a well-located area and as such accords with this SEPP.

3.2.4 Is the planning proposal consistent with applicable Ministerial Directions (s.117 directions)

In accordance with Clause 117(2) of the *EP&A Act 1979* the Minister issues directions for the relevant planning authorities to follow when preparing planning proposals for new LEPs. The directions are listed under the following categories:

- Employment and resources
- Environment and heritage
- Housing, infrastructure and urban development
- Hazard and risk
- Regional planning
- Local plan making
- Metropolitan planning

The following directions are considered relevant to the subject Planning Proposal.

Table 2 – Comparison of planning proposals with relevant Section 117 Directions

Section	Comment	Compliance
1. Employment and Resources		
Direction 1.1 – Business and Industrial Zones	<p>The planning proposal will maintain the existing zone which allows for a mix of residential and non-residential uses. The proposal will support the mixed use character of the area and the Parramatta CBD by providing dwellings close to this commercial centre as Granville is only 2km from the CBD and a short walk to the train station which provides ample connection to the CBD. The Planning Proposal will deliver homes close to employment.</p> <p>The concept reference design within the Urban Design Report supports the mixed use zoning and provides for ground floor and first level commercial/retail floorspace. The Planning Proposal will result in the site achieving the maximum permitted FSR of 6:1 which was not achievable for the land under the current DA due to the height constraint. Additional floorspace will be available for employment uses as a result which will provide the option of expanding on the amount of commercial floorspace compared to what was already approved under the existing DA.</p>	Yes
2. Environment and Heritage		
2.3 – Heritage Conservation	<p>The subject site is not of heritage interest in its own right, however is in the vicinity of two listed items in the PLEP 2011. These consist of a single storey residence at 19 East Street, and semi-detached dwellings at 21-23 East Street.</p> <p>Granville is undergoing renewal under the existing planning controls, and will continue as a result of the recommendations of the PRCUTS. A number of developments have been approved around the subject site, and it has been accepted that there will be some impact on the above mentioned heritage items.</p> <p>Specifically in relation to this Planning Proposal, the Heritage Items are separated by East Street and the impact on the items is not considered to be any more detrimental compared to the impact generated by the existing DA consent.</p> <p>In addition, the Granville War Memorial is located to the south of the subject site. The scale of the proposed development will cause some overshadowing of the item. However, given the nature of the item and the fact the proposal will generate a taller and more slender tower than what is currently approved under the existing DA, which will cast a faster moving shadow, it is considered acceptable.</p>	Yes
3. Housing, Infrastructure and Urban Development		
Direction 3.1 - Residential Zones	<p>The planning proposal is consistent with the objectives of this direction as it will increase residential densities and housing choice in a location that is close to public transport, shops, employment and recreational opportunities. The Planning Proposal will result in an additional 119 dwellings within the Granville Precinct, contributing to the delivery of the targets set within the PRCUTS. In addition, the Planning Proposal will enable the subject site to achieve its existing FSR control of 6:1 and therefore accommodate more GFA on the site than what is currently permitted under the existing DA consent. Whilst the site was consolidated within a larger site (as described above in Background), in isolation of the broader site, only achieve an FSR of 4.9:1. This Planning Proposal will</p>	Yes

	enable an additional 1.1:1 on the site than what permitted under the existing consent. With this providing the opportunity to deliver more housing.	
Direction 3.4 - Integrating Land Use and Transport	<p>Increasing the density of development within the walking catchment of transport nodes, namely the Granville Railway Station and Bus Terminal will support the viability of existing and public transport services and reduce dependence on cars.</p> <p>The subject site is within the Granville Urban Renewal Precinct which was identified under the PRCUTS due to the fact that the precinct is well serviced by transport infrastructure and has optimal access to employment opportunities. The PRCUTS also imposes lower parking rates than what is currently within the applicable Parramatta DCP 2011. The PRCUTS sets maximum parking rates, as opposed to minimum rates, and therefore will result in lower parking levels on the site under a future DA. This will contribute to the utilization of the existing transport interchange, and reduce car reliance.</p>	Yes
4. Hazard and Risk		
Direction 4.1 - Acid Sulfate Soils	<p>The Acid Sulfate Soils Map for Parramatta LEP 2011 indicates that the subject site contains Level 4 and part Level 5 acid sulfate soils. Despite this constraint, Granville has accommodated medium to high density development throughout East Street with a number of DAs already being approved for redevelopment, including the subject site. The existing Development Approval for this subject site demonstrates that any acid sulfate soil can be managed. An Acid Sulfate Soils Management Plan will be required to support any future DA in accordance with the existing provisions of PLEP 2011.</p>	Yes
Direction 4.3 - Flood Prone Land	<p>The subject site is not located in mainstream flood prone land. The localised flooding within the vicinity of the site was assessed as part of the assessment process of the previous approved development application. The assessment was carried out due to the proximity of the site to Duck Creek stormwater channel. The assessment determined that site is capable of being developed for high density mixed-use development.</p> <p>Council's Catchment Management Engineer confirmed that the flood planning matters were understood and previously addressed as part of the previous DA, and therefore any future flood requirements would be assessed in detail as part of any future DA for the subject site.</p> <p>However it was also raised that whilst the subject site is not within flood prone land, it is subject to high hazard floodwaters from localised/overland flooding.</p> <p>Whilst the Planning Proposal is not changing the existing permitted FSR (and thus gross floor area) compared to what is currently permitted on the site under the PLEP 2011, the increase in height will enable future developments to achieve the full FSR of 6:1 that is permitted on the site.</p> <p>The future DA on the site to result from the Planning Proposal will result in an increase in population compared to what has previously been approved under the exiting DA (738/2014/A). Any increase in population increases the risk of people being exposed to high hazard floodwaters from overland flow in East Street and also to the rear of the buildings.</p> <p>It is considered that evacuation is not feasible and a 'shelter in place' strategy should be pursued in the future. However, Council's Engineer was satisfied that this could be addressed</p>	Yes

	<p>as part of the future DA assessment process and Design Excellence Advisory Panel process, with the appropriate flood mitigation measures being explored at this point in time.</p> <p>Furthermore, a Site Specific Development Control Plan is recommended to be prepared to embed any specific design requirements for the flood planning / shelter in place strategy.</p> <p>Appendix 3 includes the flooding report from the previous DA.</p>	
6. Local Plan Making		
Direction 6.1 - Approval and Referral Requirements	The Planning Proposal does not introduce any provisions that require any additional concurrence, consultation or referral.	Yes
Direction 6.3 - Site Specific Provisions	<p>The Planning Proposal seeks to introduce a Site Specific Clause to exclude wintergardens (enclosed balconies) in the south-eastern corner of the site from the calculation of the future development's gross floor area calculation to manage amenity constraints from the railway line.</p> <p>This is considered acceptable given the site's location adjacent to the railway line and the need to manage the amenity constraints. Council has previously endorsed a similar clause for another Planning Proposal for land located on a major arterial road which was constrained by noise from heavy traffic. The context and location of the site at East Street warrant a similar clause to manage amenity for its future residents.</p> <p>This Clause is requested due to the current standardised definition of gross floor area which states that any enclosed balcony/wintergarden would need to be included under the gross floor area calculation. However this standard definition does not account for the need to enclose some balconies due to noise and wind conditions. Protecting balconies from noise and wind impacts is crucial on this site given the noise impacts from Parramatta Road, the railway line and the proposed building height.</p> <p>Removing wintergardens/enclosed balconies from the gross floor area will allow the inclusion of these important features of the development which make an essential contribution for future resident amenity.</p>	Yes
7. Metropolitan Planning		
7.1 Implementation of a Plan for Growing Sydney	The planning proposal is consistent with the direction. The planning proposal is consistent with the principles, directions and priorities prescribed in a Plan for Growing Sydney and this has been discussed in detail under Section B.	Yes
7.3 Parramatta Road Corridor Urban Transformation Strategy	<p>The planning proposal seeks to amend the maximum permitted building height to broadly align with the Parramatta Road Corridor Urban Transformation Strategy and to refine the FSR provisions. The planning proposal is aligned with the aims and visions set down in this Strategy.</p> <p>The planning proposal may be inconsistent with the terms of this Direction if the planning proposals is:</p> <p><i>a) Consistent with the Out of Sequence Checklist in the Parramatta Road Corridor Implementation Plan 2016 – 2023 (November 2016),</i></p> <p><i>b) Justified by a study (prepared in support of the planning proposal) that clearly demonstrates better outcomes are delivered that identified in the Parramatta Road Corridor Urban Transformation Strategy (November 2016) and the Parramatta Road Corridor Implementation Plan 2016-2023 (November, 2016) having regard to the vision and objectives,</i></p>	Yes

	<p><i>c) Of minor significance.</i></p> <p>The Planning Proposal seeks a height of 82m, which is a slight variation from the final recommendations of the Parramatta Road Corridor Urban Transformation Strategy which recommends 80m. In order to achieve a height of 82m the site will need to be removed from “Area 1” within the PLEP 2011 so it is not subject to the sliding scale prescribed under Clause 4.3(2A).</p> <p>This is supported as this variation is considered minor and does not increase the permitted yield on the site but allows for a more optimal design outcome. Council in its submission in response to the Draft PRCUTS asserted their support of greater height within Granville in order to deliver taller, slender towers to increase building separation, improve solar access, air circulation, and amenity. This proposal delivers on these design parameters, with further discussion of the Urban Design merit of the proposal is discussed above under Section C.</p> <p>It is considered that the 2m variation to the recommended height of 80m within the PRCUTS is of minor significance and has demonstrated to deliver a better outcome through the Urban Design Report that accompanies the Planning Proposal. Therefore, the Planning Proposal satisfies the parameters that Council can consider a variation to the PRCUTS under Part (5)(c) of the S117 Direction (which is outlined above).</p> <p>The planning proposal achieves the vision set out in the Strategy. The planning proposal achieves this by delivering:</p> <ul style="list-style-type: none"> • A wide range of dwelling types to suit the needs of a diverse community • New commercial/retail spaces at the ground level to generate street level activation • Public transport patronage will be enhanced focusing on a reduction of private car ownership with reduced onsite parking but increased car share facilities • High quality and highly integrated public domain and landscaping works. <p>The Planning Proposal also responds to the PRCUTS design to achieve buildings with Design Excellence. This is discussed above under the response to the Parramatta Road Corridor Urban Transformation Strategy. Furthermore further detail of how this proposal is in keeping with the vision and objectives of the PRCUTS is discussed also in this section of the proposal.</p>	
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3.3 Section C – Environmental, social and economic impact

This section considers the potential environmental, social and economic impacts which may result from the Planning Proposal.

3.3.1 Is there any likelihood that critical habitat or threatened species, populations or ecological communities, or their habitats, will be adversely affected as a result of the proposal?

The site is located within a highly modified urban environment and it is very unlikely to contain critical habitat or threatened species, populations or ecological communities, or their habitats.

3.3.2 Are there any other likely environmental effects as a result of the planning proposal and how are they proposed to be managed?

The main potential environmental impacts to be examined in detail with any future development proposal for the site are:

Heritage

The subject site does not contain a heritage item listed under PLEP 2011 and does not directly adjoin any items of heritage significance. However the site is in the vicinity of two heritage listed items.

- Heritage Item - 106 at 19 East Street is a representative example of a modest late Victorian house.
- Heritage Item – 107 at 21-23 East Street includes Victorian semi-detached houses and both contribute to the character of the streetscape.

Granville is undergoing renewal under the existing planning controls, and will continue as a result of the recommendations of the PRCUTS. A number of developments have been approved around the subject site, and it has been accepted that there will be some impact on the heritage items. Specifically in relation to this Planning Proposal, the Heritage Items are separated by East Street and the impact on the items is not considered to be any more detrimental compared to the impact generated by the existing DA consent. The Granville War Memorial is located to the south of the subject site. The scale of the proposed development will cause some overshadowing of the item. However, given the nature of the item and the fact the proposal will generate a taller and more slender tower than what is currently approved under the existing DA, which will cast a faster moving shadow, it is considered acceptable.

A heritage statement is found in Appendix – 5.

Urban Design

The Planning Proposal seeks to increase the permitted height from 52m to 82m which is considered to provide sufficient height to deliver the permitted FSR of 6:1. As discussed above, the recent DA assessment for the subject site demonstrated that under the existing height control, an FSR of 6:1 cannot be achieved. The floor space that was approved was to be delivered in a building massing that was bulky and had a large floorplate. This was not considered an good built form outcome.

The Planning Proposal will permit more height which will deliver a taller and more slender tower with increased building separation, improved solar access, air circulation, and amenity. Increasing the height on the subject site will provide a variation in the tower forms and massing along East Street. As seen in Figure 2 and Table 1 of this proposal, the majority of the East Street has already been approved for development under the constricted height control, which has resulted in shorter and bulkier tower floorplates. The increase in height will provide variation in the massing and streetscape and result in a better built form outcome along East Street. Figure 6 is an extract from the Urban Design Report that accompanies the Planning Proposal. This compares the approved DA scheme for the subject site with the indicative massing to result from the

Planning Proposal, and shows how an increase in height on the subject site (i.e. the building in yellow) will deliver a tower with a smaller floorplate, improve solar access, provide variation to the building heights along East Street to break up the built form, and result in an improved urban design outcome.



Figure 1 – Approved DA North Elevation



Figure 2 – Proposed additional height building envelope

- 1 18m gap between towers
- 2 Variation to skyline silhouette and creation of landmark tower adjacent rail station
- 3 Podium architecture to reflect streetscape character of surrounding approved developments – See examples on the following page

Figure 6 – Extract from Urban Design Report showing approved DA and Planning Proposal scheme

In summary the Planning Proposal will result in a better built form and urban design outcome on the subject site. Given the scale of development currently permitted under the PLEP 2011 for Granville (which is further reinforced by the recommendations of the PRCUTS), ensuring new development exhibits design excellence and a built form outcome that complies with the Apartment Design Guidelines (SEPP 65) is crucial. This is addressed within the SEPP table below, however the concept design is considered to broadly comply with the design and amenity considerations of SEPP 65 (which will be further explored at the DA stage).

The Urban Design Report and Concept Reference Design is found in Appendix 1.

Amenity

The subject site is located adjacent to the Western Railway Line. The concept reference design proposed to manage amenity constraints through urban design initiatives. Whilst these will be explored in detail as part of a future development application, the concept reference design submitted with this Planning Proposal proposes the use of wintergardens (enclosed balconies) in the south-eastern corner of the development to manage the constraints.

As a result, the Planning Proposal seeks to introduce a Site Specific Clause to exclude wintergardens (enclosed balconies) from the calculation of the future development's gross floor area calculation. This is considered acceptable given the site's location adjacent to the railway line and the need to manage the amenity constraints. The

context and location of the site at East Street warrant a similar clause to manage amenity for its future residents.

Transport and Accessibility

The Planning Proposal is in close proximity to the Granville Transport Interchange which offers connection to both the Parramatta CBD and the Sydney CBD. The Western Railway line provides frequent services along the corridor, offering the community superior connectivity to employment opportunities.

The Planning Proposal as discussed above in relation to the PRCUTS, propose to introduce the maximum car parking rates recommended within the PRCUTS. This will ensure the amount of additional traffic is reduced compared to what would be permitted under the current controls in the Parramatta DCP 2011. The current controls permit minimum car parking rates. Whilst these rates will be included within a Site Specific DCP for the subject site, the DCP is being prepared in response to the lodged Planning Proposal, and will result in fewer spaces on the site.

A traffic statement is found in Appendix 4.

3.3.3 How has the planning proposal adequately addressed any social and economic effects?

There is adequate justification for this planning proposal, which will facilitate an increase in density of housing and provide some employment opportunities.

It is noted that the majority of the Gross Floor Area to be generated on the subject site as indicated in the reference design is residential. The dominant residential use will deliver a range of housing options located in close proximity to public transport, employment and community facilities. Furthermore it will assist in the delivery of the dwelling target for Granville as shown in the PRCUTS.

The non-residential floorspace will contribute to the provision of retail and commercial uses that support the current and anticipated population in Granville. The provision of this type of floorspace on the ground and podium level will provide the opportunities for doctors, accountants and similar uses to be located in close proximity to public transport and the growing community of Granville.

3.4 Section D – State and Commonwealth Interests

3.4.1 Is there adequate public infrastructure for the planning proposal?

As discussed within this proposal, the subject site is located within the Granville Precinct identified under the Parramatta Road Corridor Urban Transformation Strategy. This strategy aims to deliver around 5,400 new dwellings in Granville. As a result of this target, studies were carried out to identify the infrastructure requirements of the area to ensure the new community is adequately serviced. The PRCUTS details a comprehensive list of infrastructure to accommodate Granville precinct. The Infrastructure Schedule includes traffic and transport improvements, new and embellished open space and social infrastructure and identifies section 94 or 94A contributions plans and VPAs to fund local infrastructure.

The intention of this planning proposal (as discussed in this document) is to provide a height that will a) enable the delivery of the existing FSR of 6:1 and b) provide a better built form outcome. The permitted density on the site is not proposed to be increased compared to what is already permitted under the PLEP 2011. Therefore, additional infrastructure contributions have not been sought other than what will be required under the Development Contribution Plan that applies to the subject land.

3.4.2 What are the views of State and Commonwealth public authorities consulted in accordance with the gateway determination?

Early engagement with the Roads and Maritime Services (RMS) has been carried out due to previous discussions had with RMS relating to the management of traffic within Granville in light of the additional dwellings planned for within the PRCUTS.

The PRCUTS states that:

“prior to any rezoning commencing, a Precinct-wide traffic study and supporting modelling is required to be completed which considers the recommended land use and densities, as well as future Westconnex conditions, and identifies the necessary road improvements and upgrades required to be delivered as part of any proposed renewal in the Precinct”.

Council wrote to the RMS to explain the nature of the proposal and the fact that the permitted FSR (i.e. density) on the subject site is not proposed to be changed under the Planning Proposal. Rather the proposal seeks to increase the height broadly consistent with the PRCUTS to change the distribution of the existing permitted FSR on the subject site to deliver a more appropriate built form outcome.

The precinct wide traffic study has not yet commenced. However it is considered appropriate for this proposal to proceed in the absence of the precinct wide traffic study as the FSR is not proposed to be changed. The engagement was to explain the background and intent of the proposal early in the planning process given the PRCUTS specifies that a traffic study needs to be completed before the PRCUTS can be implemented in Granville.

Should the Planning Proposal receive a Gateway determination, another referral will be sent to RMS as part of any future public exhibition required under Section 56(2)(d) of the *Environmental Planning and Assessment Act 1979*. Therefore, RMS will be provided with another opportunity to comment on the proposal during the statutory exhibition of the planning proposal should a Gateway determination be issued.

PART 4 – MAPPING

This section contains the mapping for this planning proposal in accordance with the DP&E's guidelines on LEPs and Planning Proposals.

4.1 Existing controls

This section contains map extracts from *PLEP 2011* which illustrate the current controls applying to the site.

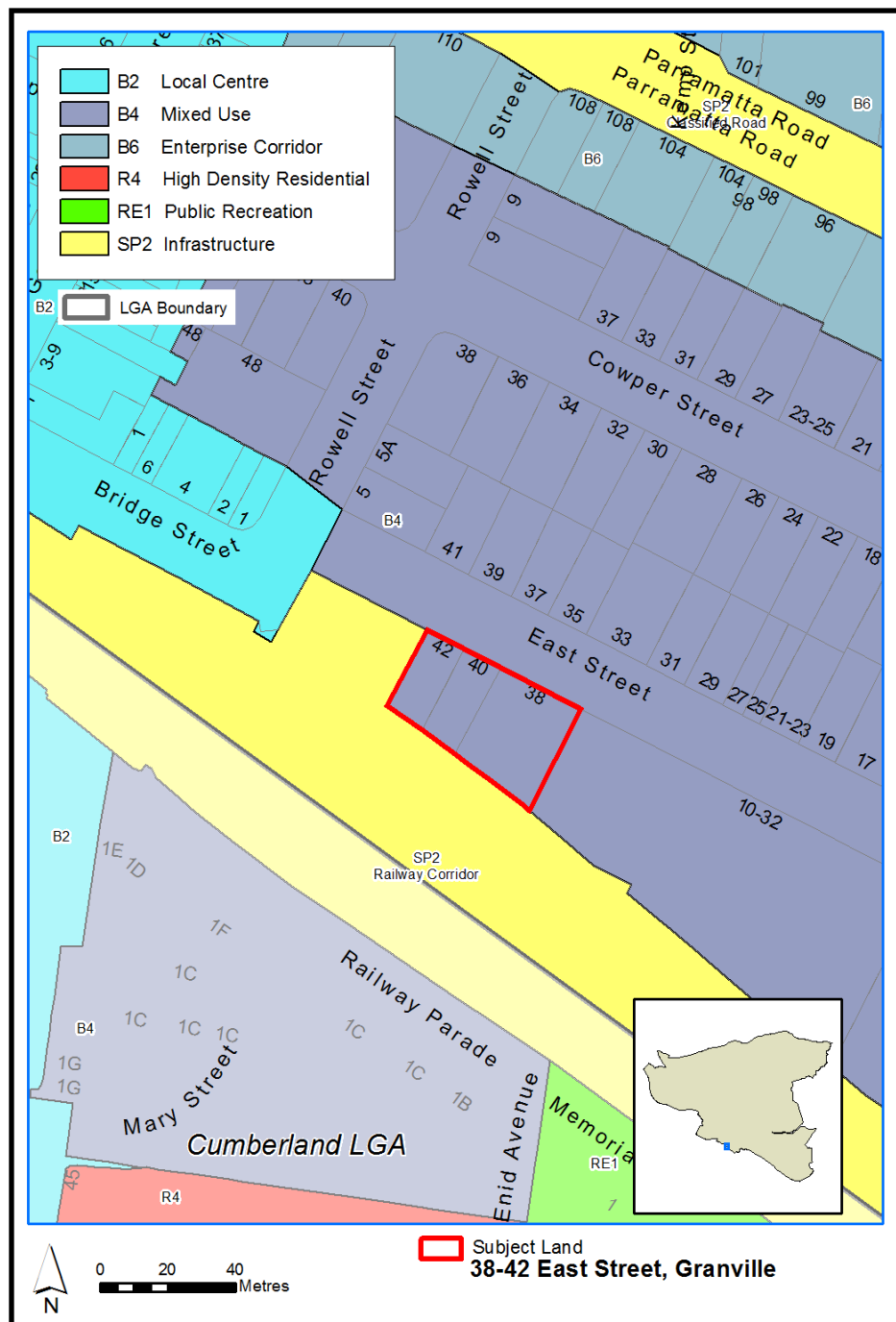


Figure 7 Existing zoning extracted from the *PLEP 2011* Land Zoning Maps

Figure 7 above illustrates the existing B4 Mixed Use zone over the site. There is no proposed change to the zone.

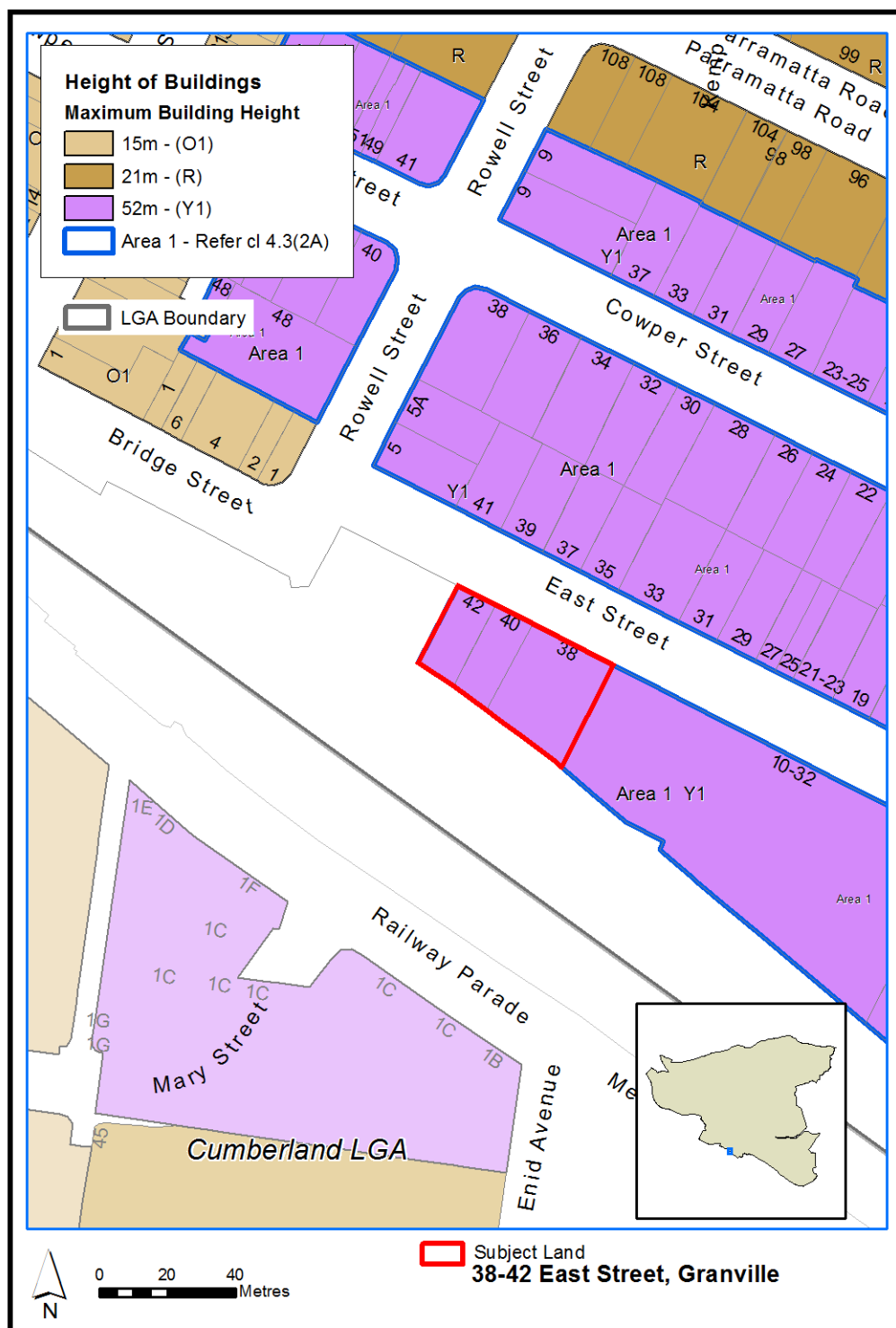


Figure 8 – Existing building heights extracted from the PLEP 2011 Height of Buildings Maps

The subject site is located within “Area 1” of the HOB map which is subject to Clause 4.3(2A) of the PLEP 2011 (see **Figure 8**). This clause stipulates the way heights are calculated on the land demarked as “Area 1”, and applies a sliding-scale process of height allocation. To achieve the maximum building height of 52 metres presented on the Height of Buildings Map, sites within “Area 1” must have a land area greater than 3,200m². This is to encourage site amalgamation and deliver well-designed built form outcomes. The site has an area of 1577m²

which is greater than 950m² and less than 2,100m², therefore under the provisions of Clause 4.3(2A) a height of 21m is permitted.

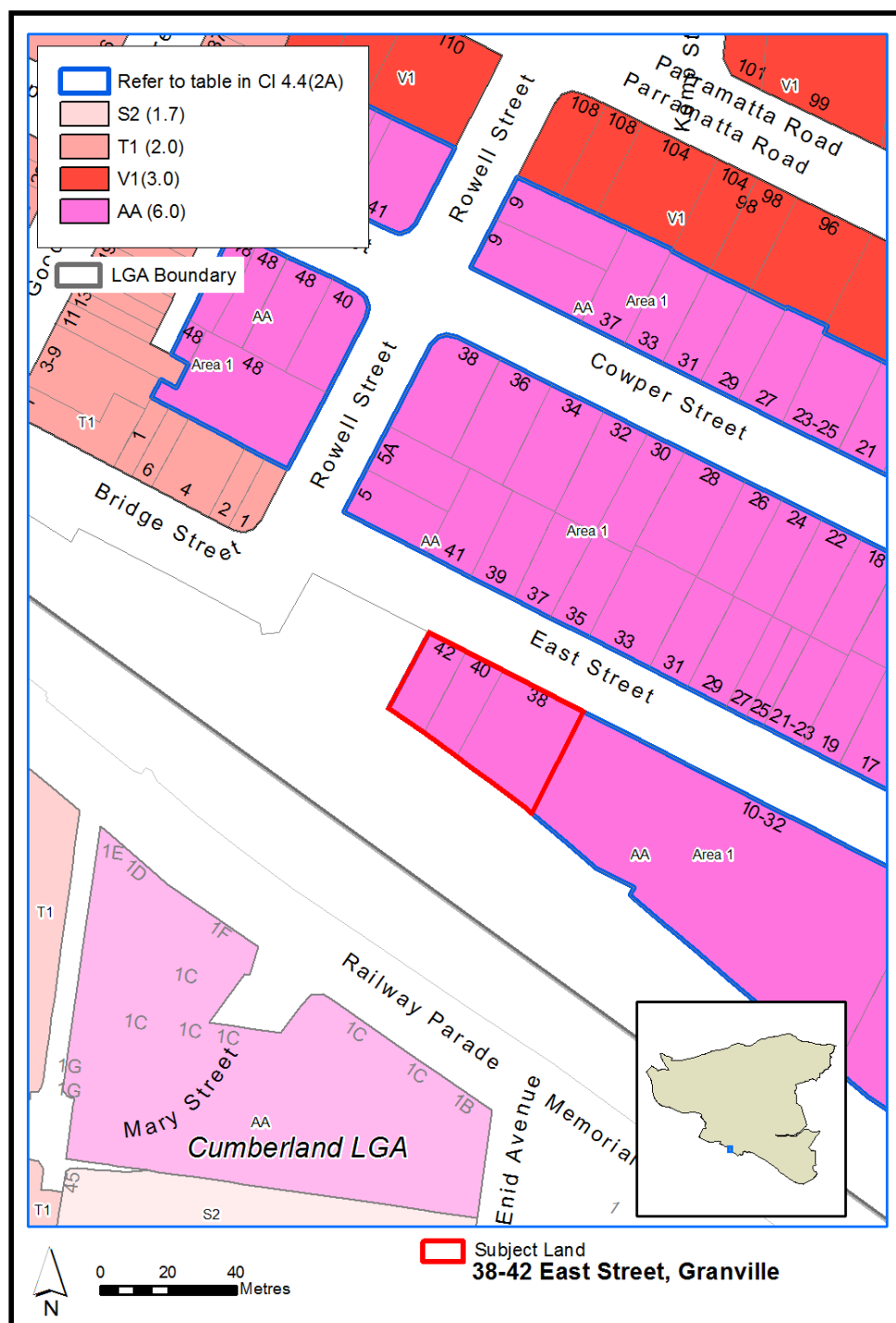


Figure 9 – Existing floor space ratio extracted from the *PLEP 2011 Floor Space Ratio Map*

The land is within “Area 1” of the Floor Space Ratio map which is subject to Clause 4.4(2A) of the PLEP 2011 (see **Figure 9**). This clause stipulates the way FSR is calculated on the land demarked as “Area 1”, and applies a sliding-scale to determine the FSR that applies to the site. To achieve the maximum FSR of 6:1 presented on the Floor Space Ratio Map, sites within “Area 1” must have a land area greater than 3,200m². This is to encourage site amalgamation and deliver well-designed built form outcomes. The subject site has a site area

of 1577m² which is greater than 950m² and less than 2,100m², therefore under the provisions of Clause 4.4(2A) an FSR of 3.5:1 is permitted.

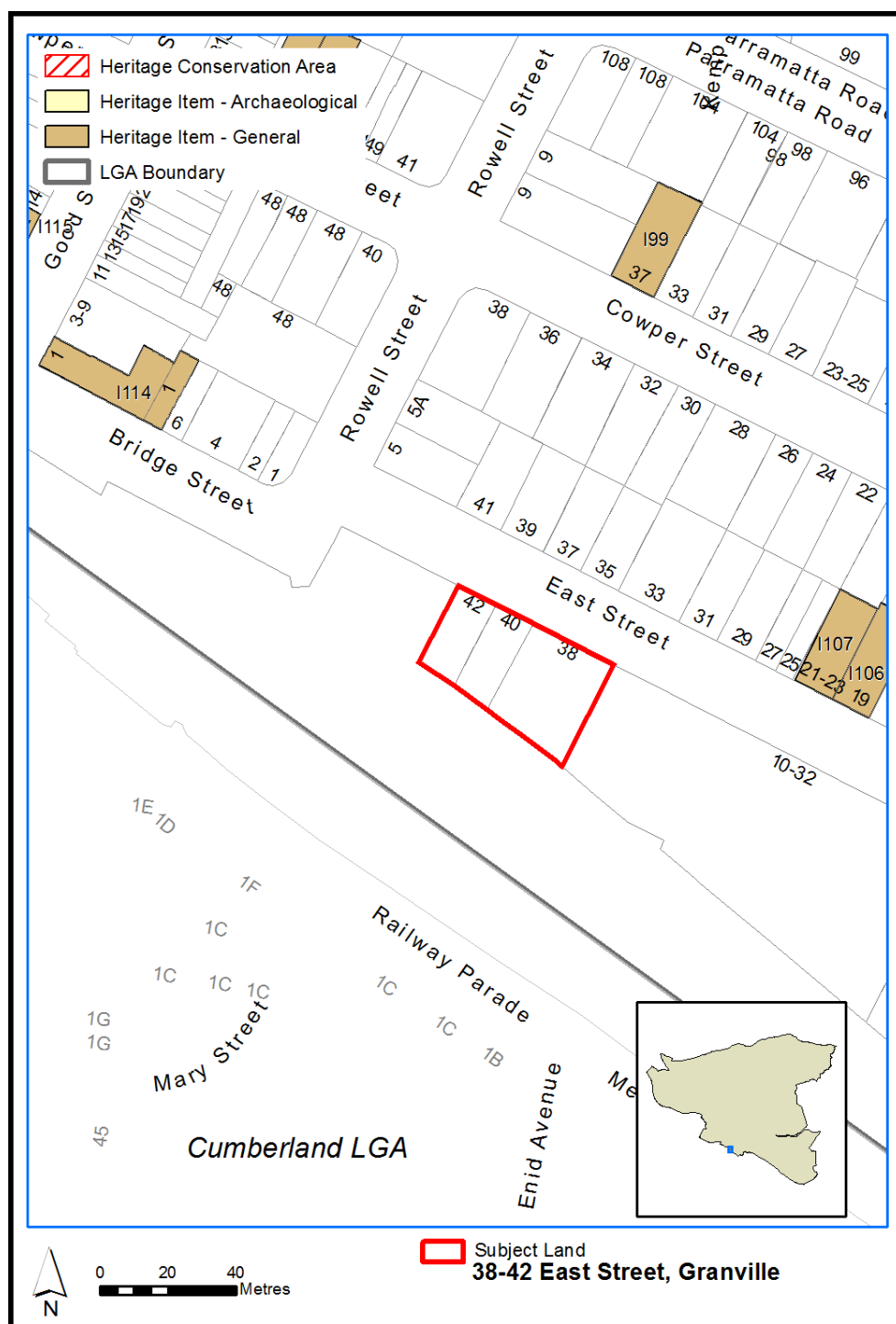


Figure 10 – Existing heritage items extracted from the PLEP 2011 Heritage Maps

Figure 8 above illustrates the heritage items in the locality.

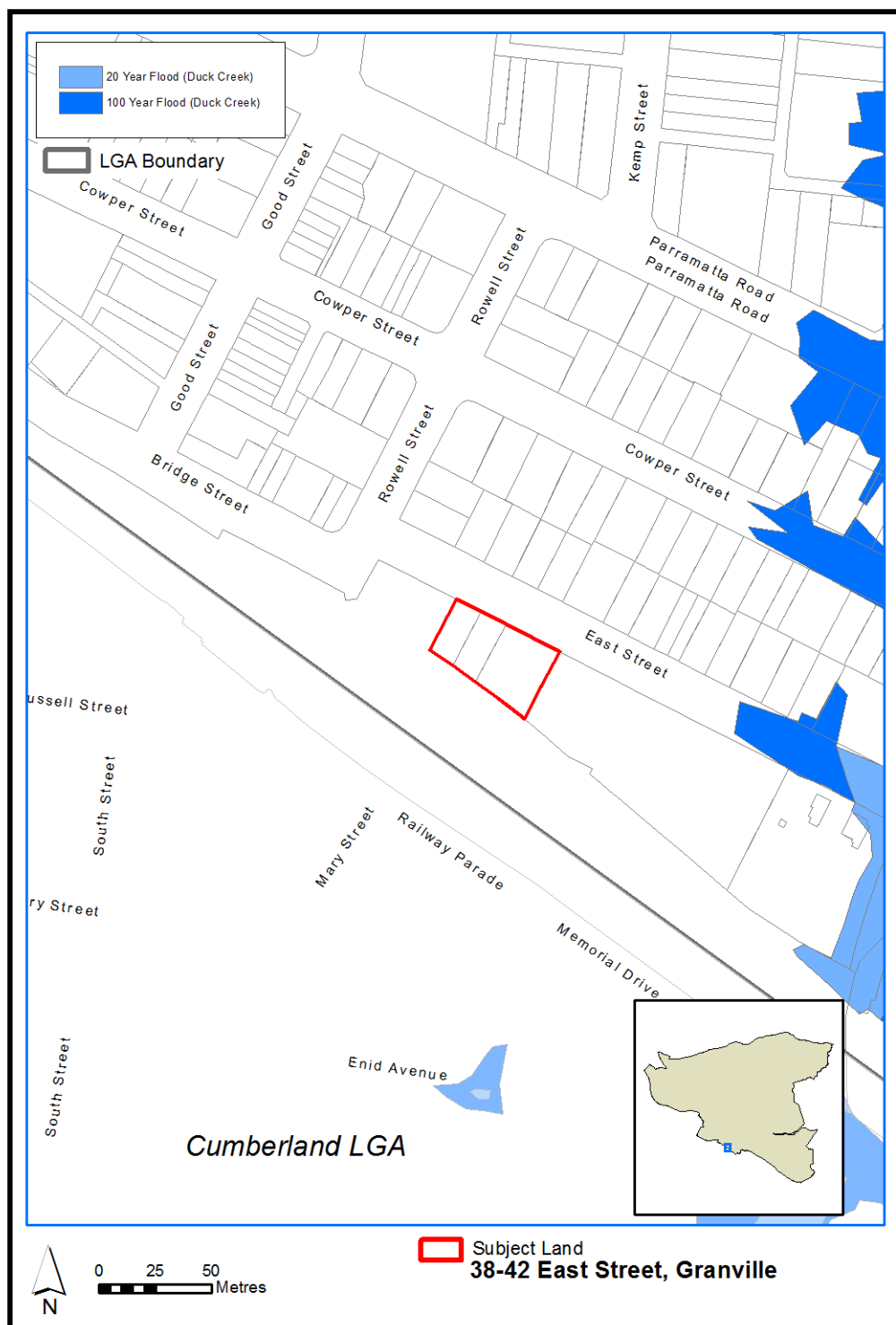


Figure 11 – Existing flooding extent

Figure 11 above illustrates the site is not affected by mainstream flooding.

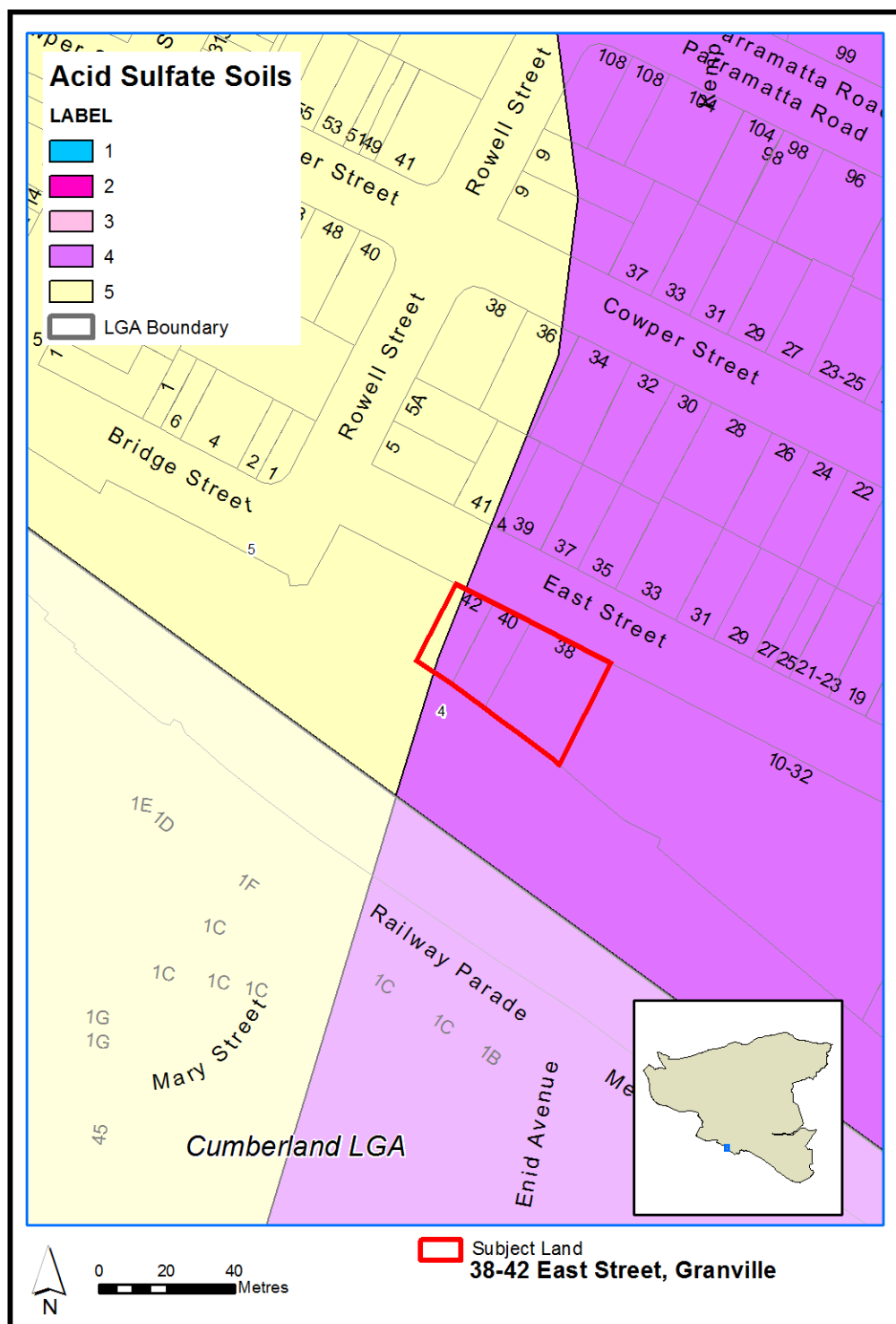


Figure 12 – Acid Sulphate Soils extracted from the *PLEP 2011* Acid Sulphate Soils Maps
Figure 12 shows the site is effected by Level 4 and 5 Acid Sulphate Soils.

4.2 Proposed controls

The figures in this section (Figures 13 and 14) illustrate the proposed building height and floor space ratio controls sought by this planning proposal.

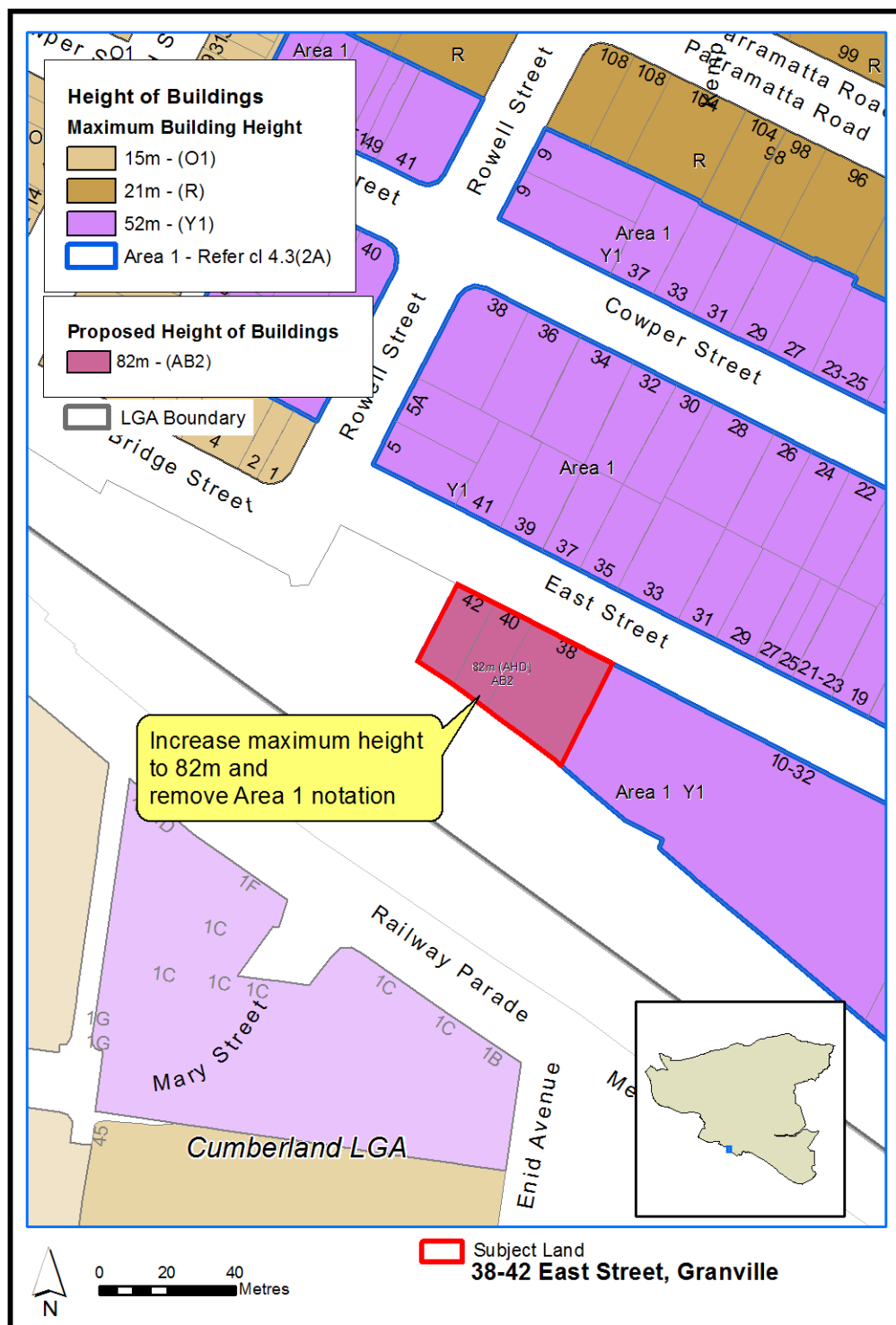


Figure 13 – Proposed amendment to the *PLEP 2011* Height of Building Map

It is proposed that the Maximum Height of Building control be increased from 52m to 82m, and that the site be removed from 'Area 1' on the Height of Building Map to remove the application of the sliding scale provision under Clause 4.3(2A) of the PLEP 2011 as shown in **Figure 13**.

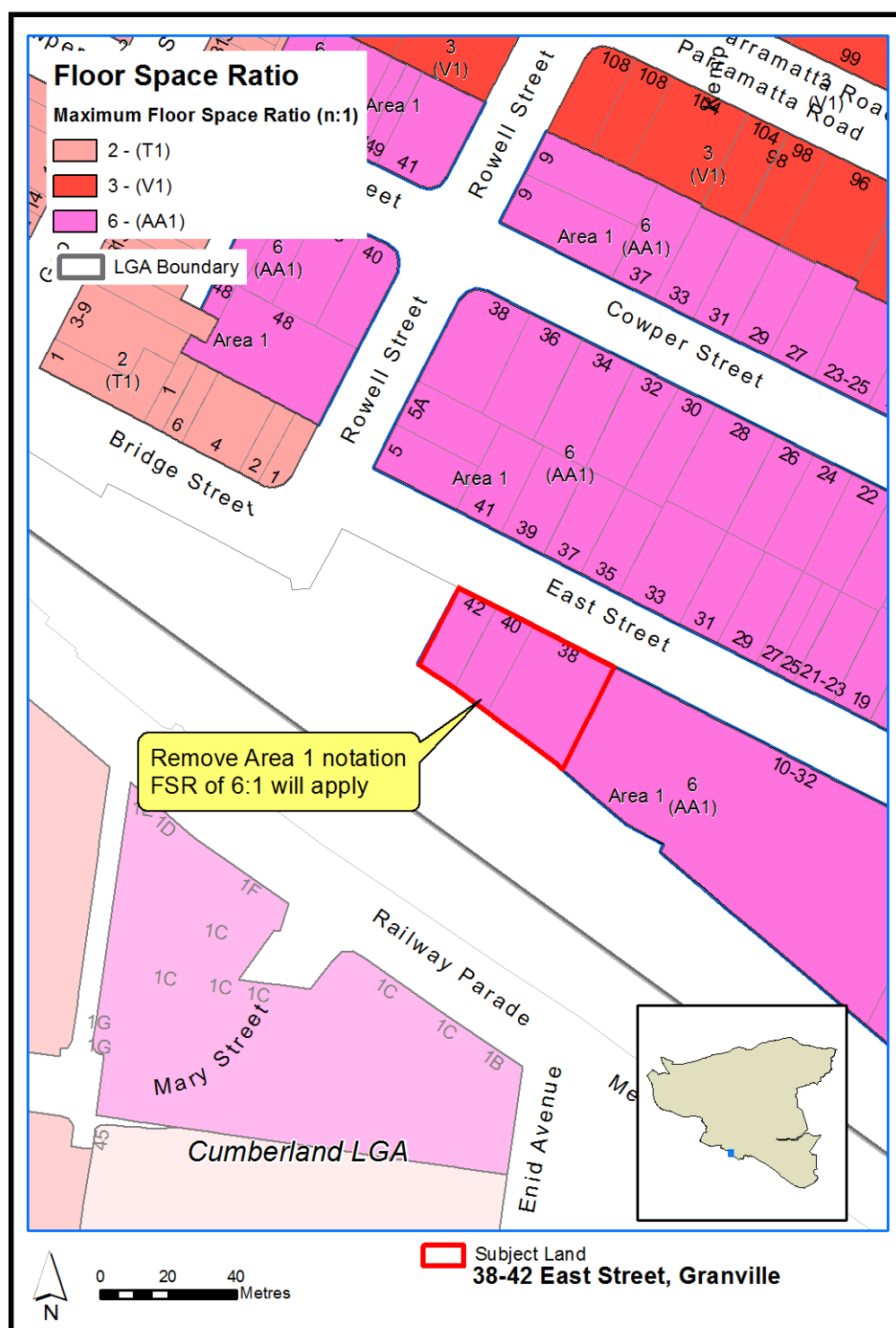


Figure 14 – Proposed amendment to the PLEP 2011 Floor Space Ratio Map

It is proposed that the existing 6:1 Maximum Floor Space Ratio control be retained, however for the site to be removed from 'Area 1' on the FSR map to remove the application of the sliding scale provisions within Clause 4.4(2A) of the PLEP 2011 as shown in **Figure 14**.

PART 5 – COMMUNITY CONSULTATION

In accordance with Section 57(2) of the *EP&A Act 1979*, the Director-General of Planning must approve the form of the planning proposal, as revised to comply with the gateway determination, before community consultation is undertaken.

Public exhibition is likely to include:

- newspaper advertisement;
- display on the Council's web-site; and
- written notification to adjoining landowners.

The gateway determination will specify the level of public consultation that must be undertaken in relation to the planning proposal including those with government agencies.

Pursuant to Section 57(8) of the *EP&A Act 1979* the Responsible Planning Authority must consider any submissions made concerning the proposed instrument and the report of any public hearing.

PART 6 – PROJECT TIMELINE

The detail around the project timeline is expected to be prepared following the referral to the Minister for a Gateway Determination.

The following steps are anticipated:

- Referral to Minister for a Gateway determination (October 2017)
- Commencement and completion dates for public exhibition period and government agency notification (December 2017)
- Consideration of submissions (February 2018)
- Consideration of proposal post exhibition and reporting to Council (March 2018)
- Submission to the Department to finalise the LEP (April 2018)
- Notification of instrument (May 2018)

Appendix 1 – Urban Design Report and Concept Reference Design

34-42 East St Granville

Urban Context and Design Response Report – Rev 4 - 30 August 2017



DPG6C 34-42 East St – Planning Proposal Urban Context and Design Response

1.0 Introduction

This report has been prepared by Develotek Property Group's Design & Planning Team.

The purpose of this report is to:

- Establish the urban & planning context applicable to the site
- Compare the existing LEP provisions with the *Parramatta Road Corridor Urban Transformation Strategy* (The Strategy)
- Document a proposed response to the context to realise the objectives of *The Strategy*

Section 2.0 establishes the strategic context for the subject site

Section 3.0 establishes the existing and proposed planning context for the site

Section 4.0 documents the proposed built form outcome for the site

Section 5.0 Conclusion

Appendix A Architectural reference design

Appendix B Podium Design

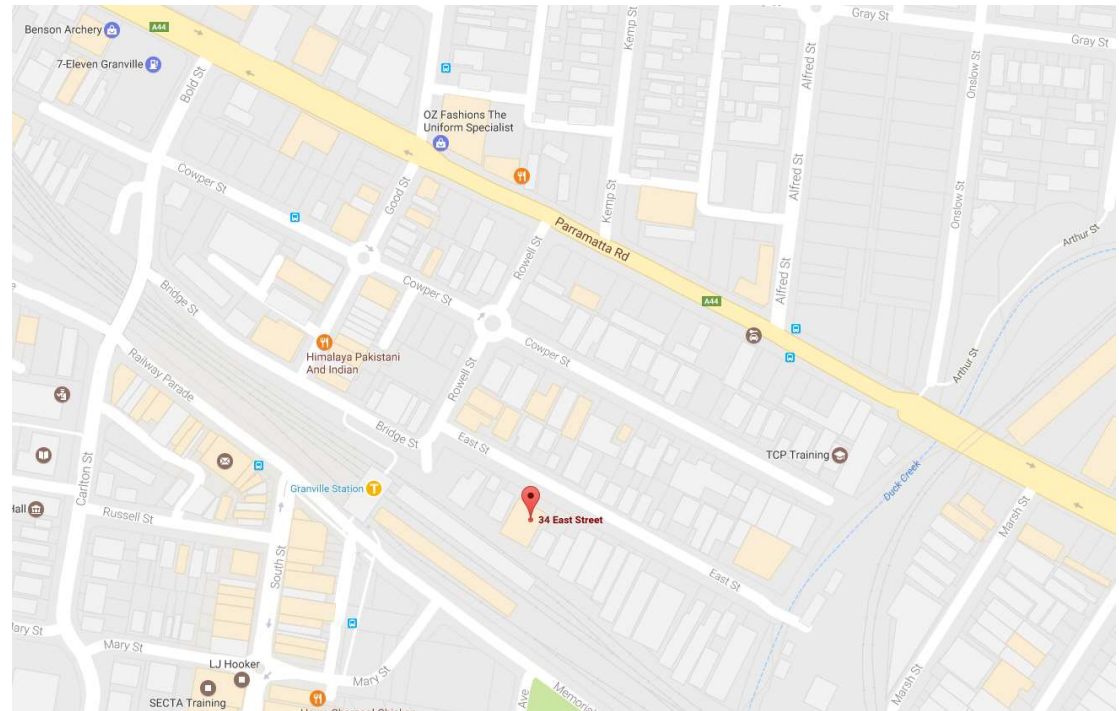
Appendix C Development Data

The subject site known as DPG6C is located at 34-42 East St Granville and includes the following properties:

Lot 1 DP 1009146

Lot 1 DP 195784

Lot 1 DP 996285



SITE LOCALITY MAP

Source: Google Maps

2.0 Strategic Context

The Granville Precinct is located approximately 1.5 kilometres south east of the Parramatta CBD and immediately north of the existing highly active Granville town centre and Granville rail station. It spans both sides of Parramatta Road to the north and south, and is bounded to the north by Boundary Street and the M4 Motorway, and the Western Rail Line to the south. Woodville Road/Church Street mark the western boundary and Duck Creek is the Precinct's eastern boundary.

The Granville precinct has been identified in the *Parramatta Road Corridor Urban Transformation Strategy* as an area with significant opportunity to leverage off its strategic location between Parramatta CBD and the existing Granville Town Centre located to the south of Granville Rail Station.

The Strategy outlines a vision for Granville as a vibrant mix of new housing, shops and commercial spaces, linked by a much improved network of streets and attractive new parks and public spaces.

The subject site at 34-42 East St is located in a mixed use zone, within 100m walk to the Granville Station, 200m from Parramatta Rd, and within 2km of the Parramatta CBD.

This planning proposal contributes to the realisation of the following opportunities identified by *The Strategy*:

- high accessibility to employment, recreation, entertainment and cultural facilities in the Parramatta CBD
- potential to extend the existing Granville town centre north and provide commercial and retail floor space to accommodate additional urban services such as supermarkets, day-to-day business services, indoor recreation opportunities and child care facilities
- presence of distinct employment uses across the Precinct and Frame Area including Auto Alley adjoining the Parramatta CBD and the Mort Street and Clyde employment lands
- celebrating Granville's identity as a 'destination' for food by providing opportunities for restaurant space and outdoor dining
- good proximity to heavy rail and bus services
- relatively permeable blocks and wide streets which provide a strong framework to deliver high quality public domain and green links, high levels of activation and a pedestrian friendly environment
- a high degree of development activity around the railway station that presents the opportunity to achieve transformation with quality and improved built form outcomes.

This planning proposal responds to the following constraints identified by *The Strategy*:

- large volumes of cars and heavy vehicles traverse this section of Parramatta Road which provides a north-south connection between Woodville Road and James Ruse Drive. Due to its location directly adjacent the Granville rail station, an opportunity exists to encourage green travel and reduced car ownership and use, minimising additional impact on the existing road network.
 - limited north – south connections across Parramatta Road and the Rail Lines which provide barriers for both vehicles and pedestrians. The current Development Consent for the site allows for a future pedestrian connection across the rail line. The proposed site has potential to address this future connection, ensuring passive surveillance and security.
 - long blocks (>200m in places), small lot sizes and land fragmentation which could limit the ability to achieve through links and further improve permeability. This proposal seeks additional height to respond to the small lot size and achieve the potential 6:1 FSR adjacent the Granville Rail Station.
 - flooding along Duck Creek. The current Development Consent demonstrates suitable flood mitigation measures are possible for the site.
- This planning proposal contributes to the realisation of the vision outlined in *The Strategy* by:
- improving current height and density controls to ensure good built form outcomes are achieved. Increasing the height control in line with *The Strategy* allows for a slender tower clearly delineated from a podium base. This results in improved streetscape experience for pedestrians while creating interest in the skyline and defining a landmark at Granville Station.
 - recognising Granville as one of Sydney's oldest suburbs by preserving the fine grain rhythm on Good Street and incorporating heritage buildings and streetscapes into new development across the Precinct. Ground floor retail uses will continue the fine grain rhythm of Good St and respect existing heritage elements on East St.
 - creating better links between sites and delivering new connections and upgrades to facilitate improved access and movement, making it easier for people and cars to move north to south and cross major roads and the railway lines. There is an opportunity for the ground floor to open up and address the future pedestrian link between 32 & 34 East St ensuring permeability, safety and security.
 - mitigating the impact of noise from busy roads and the rail lines in residential areas. The proposed 4 storey podium mitigates visual privacy and acoustic impacts from the Rail corridor & station.
 - responding to small lot sizes and land fragmentation by developing built form controls which encourage lot amalgamation. The proposal takes advantage of the current Development Consent to maximise the potential of a small lot size adjacent the Granville Rail Station.



Good St Retail



Granville Station (Corner Bridge St & East St)

DPG6C 34-42 East St – Planning Proposal Urban Context and Design Response

2.1 Immediate Urban Context

The immediate locality is characterised by traditional one and two storey commercial buildings particularly near Granville Railway Station and along Good and Bridge Streets with light industrial land uses situated on the eastern and western edges of the centre. Commercial and industrial development is interspersed by low to medium density residential dwellings. Land use along Parramatta Road is dominated by car dealerships. The varied nature of land use in the locality has resulted in an urban form that lacks cohesion and sense of place.

This locality is undergoing a significant period of change consistent with the site's proximity to Parramatta and its role as Sydney's second CBD. The Parramatta Road corridor is planned to become a strategic centre for employment and high density housing. This location is well serviced by public transport and is close to the existing Granville retail and business centre which is on the southern side of the railway line. The site is also within 2km walking distance of the Parramatta CBD. This offers future residents significant convenience and results in land that is prime for redevelopment for high density residential.



Figure 1 – Existing view east along East St showing the Granville Rail Station and associated buildings in the foreground and the 19 storey development at 2-8 East St (recently completed) in the background



Figure 2 – Existing view east along East St showing low rise industrial & residential uses



Figure 3 – View of subject site from East St showing low rise warehouse type uses and rail corridor visible in the background

3.0 Current approved DA design

Consent under DA/738/2014 exists for a 19 storey tower 'Block A', a 17 storey tower 'Block B' and a 17 storey tower 'Block C' on the subject site.

Since the original consent, blocks A & B (10-32 East St) have been sold to a developer and are currently under construction.

Figures 1 & 2 below show the current approved scheme for the site. Blocks B & C are a similar height. Podiums levels are differentiated from the towers by changes in balcony form and materiality, with street facing balconies roughly in line with the podium.

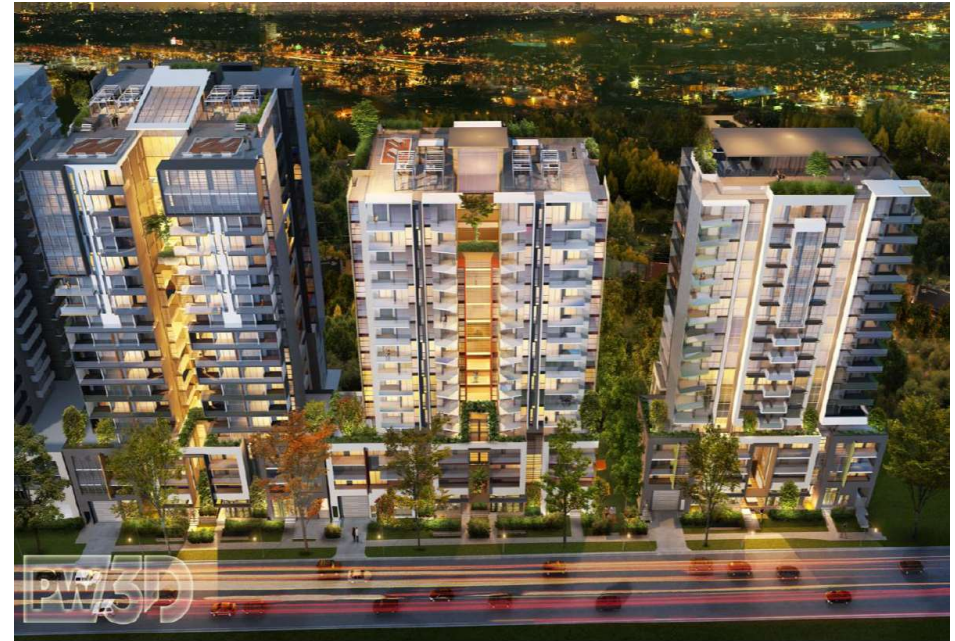


Figure 1 – Photomontage (Original DA)

Figure 2 – East St Elevation (Original DA consent)



DPG6C 34-42 East St – Planning Proposal Urban Context and Design Response

3.0 Current approved DA design

Figures 1 & 2 below excerpted from the current approved DA indicate the relationship between the Granville Train Station and the proposed tower.

An opportunity exists to increase podium to tower setbacks, and improve the interface between the podium levels and the train station to reduce acoustic impacts and enhance privacy of residential apartments.

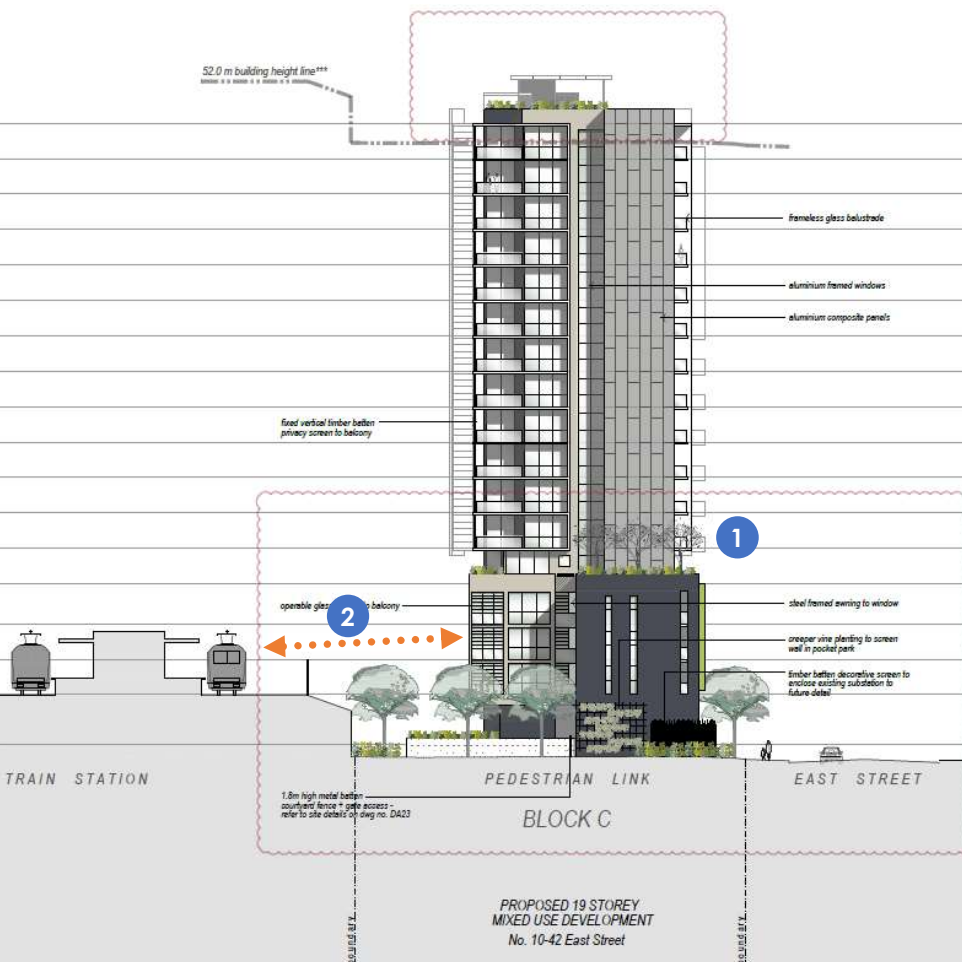


Figure 1 – Current approved Block C East Elevation
Excerpt from DA consent documentation



Figure 2 – Current approved Block C West Elevation
Excerpt from DA consent documentation

3.0 Current approved DA design

The below figure is an excerpt from the approved Development Application showing cross sections through tower C

There is typically a 2m setback from podium levels to the main tower form above, with balconies protruding within this setback.

There is an opportunity with Block C to create a clearly defined podium to tower setback of 3m, achieving the objectives of the built form guidelines for podium / tower typology in *The Strategy*.

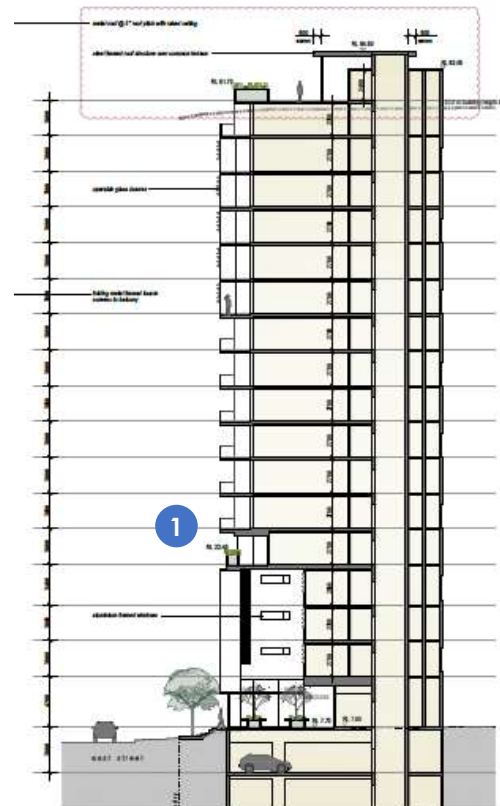


Figure 1 – Current approved Block C cross section
Excerpt from DA consent documentation

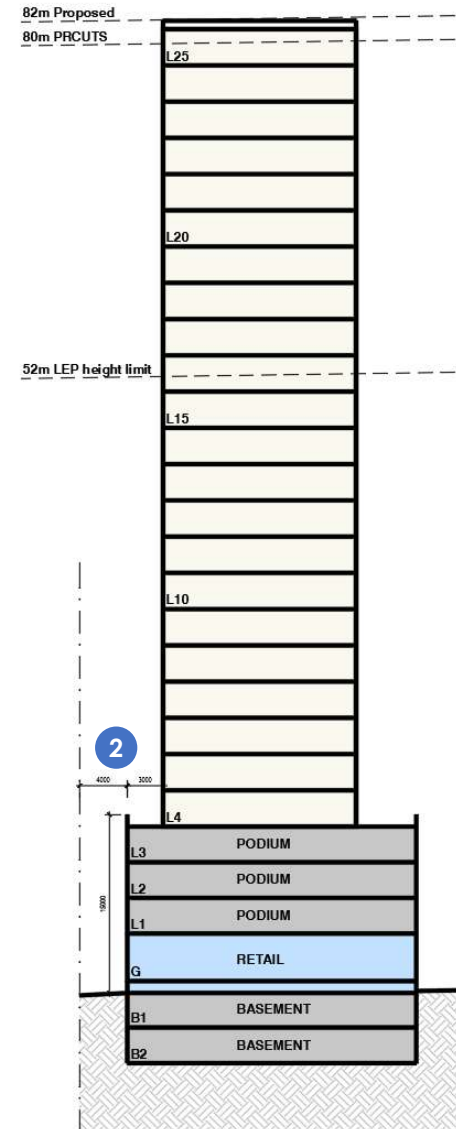


Figure 2 – Indicative potential Block C cross section

- 1 Balconies aligned to podium below
- 2 Clear 3m setback from podium to tower

DPG6C 34-42 East St – Planning Proposal Urban Context and Design Response

3.1 Existing Planning Controls

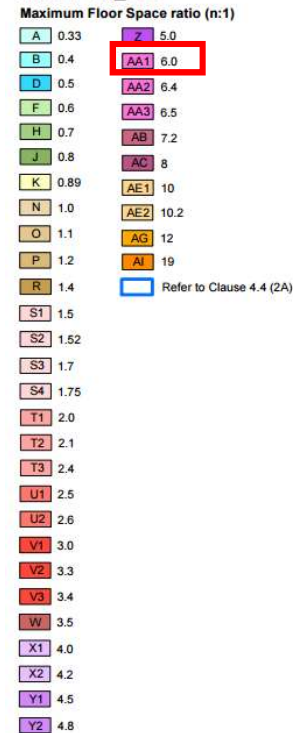
LEP 2011

Currently, Parramatta's Local Environment Plan 2011 (LEP) prescribes a Floor Space Ratio (FSR) of 6:1 with a height limit set at 52m.

Current Approval

The approved development at 10-42 East St (including the subject site) complies with these existing controls but results in a continuous wall of short, dense towers at 17-19 storeys.

Floor Space Ratio Map -
Sheet FSR_010



Maximum Building Height (RL)
Heights shown on map in RL (m)

RL 11
RL 14

Maximum Building Height (m)

E	6	Y1	52
G	7	Y2	54
J1	9	AA1	60
J2	9.2	AA2	66
J3	9.5	AA3	72
K	10	AB1	80
L	11	AB2	90
M	12	AC1	118
N1	13	AC2	120
N2	14	AD1	130
O1	15	AD2	136
O2	16	AE	150
P1	17	AG	200
P2	18	Area 1	Refer to table in Clause 4.3 (2A)
Q1	19	Area 2	Refer to Clause 7.4
Q2	20		
R	21		
S	24		
T1	25		
T2	28		
U1	31		
U2	34		
V1	36		
V2	37		
W	40		
X1	48		
X2	49		

3.2 Existing Planning Controls

DCP 2011

Currently, Parramatta's Development Control Plan for the Granville precinct prescribes a 3m building setback to East St, and encourages height and density in line with the LEP.

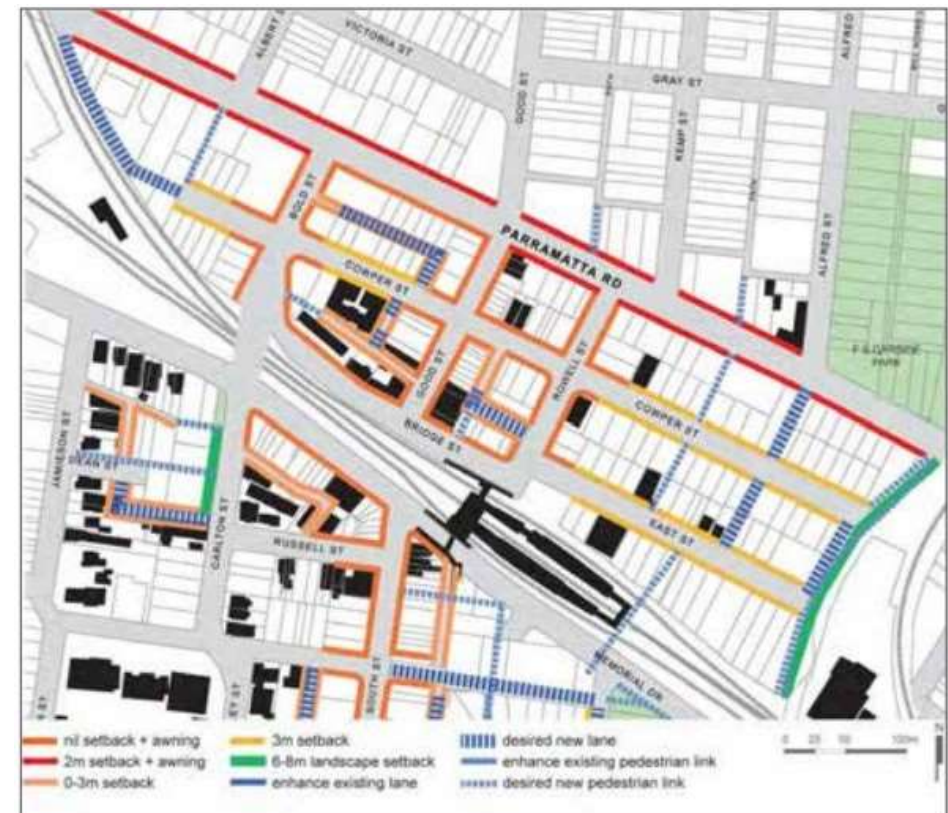
Section 4.1.6 of the DCP states the following relevant key objectives:

O.1 – To ensure that new development provides a strong interface to Granville Railway Station.

O.3 – To ensure that new development responds well to existing heritage items.

O.4 - To ensure new development within the mixed use area provides active ground floor uses to increase the safety, use and interest of the area.

O.5 - To ensure new buildings within the mixed use area provide articulation and an attractive composition of building elements.

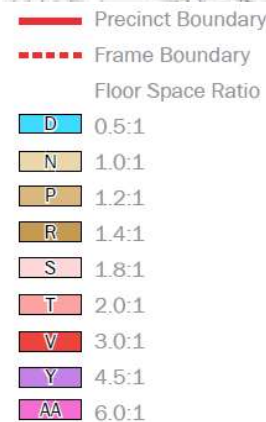


3.3 Proposed Planning Controls

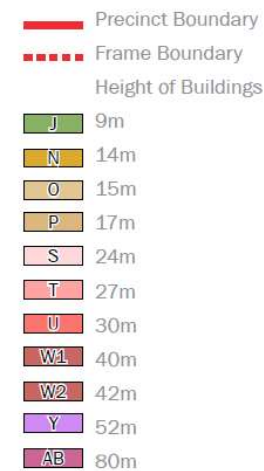
The Parramatta Road Corridor Urban Transformation Strategy recommends maintaining 6:1 FSR for the subject site, and increasing the allowable height from 52m to 80m.

This presents an opportunity to design a smaller tower floorplate with a clear setback from a low rise podium achieving a better streetscape outcome for pedestrians, while increasing the gap between towers, and providing a more interesting skyline that will be highly visible from the south of the rail corridor.

This Planning Proposal seeks to increase the height limit by a further 2m to allow for effective flood mitigation measures at ground level, and an increased typical floor to floor height recommended in *The Strategy*.



PRCUTS Excerpt Figure 5.14
Granville Recommended Building Densities



PRCUTS Excerpt Figure 5.13
Granville Recommended Building Heights

4.0 Built Form Principles

The following section demonstrates a built form responsive to Part 4 of the *Parramatta Road Corridor Urban Transformation Strategy* based on the following principles:

Podium Level Street Setback

- Set back the lower 4 storeys 3-4m, consistent with the DCP and Strategy
- Continue the existing street wall created by adjacent approved development
- Define the East St 'Street Edge' with a 4 storey podium less than 18m high

Street Activation

- Maximise Retail frontage at ground level to activate streetscape
- Create fine grain retail character through multiple entries and articulation
- Ensure pedestrian safety and accessibility

Podium Level Rear & Side Setbacks

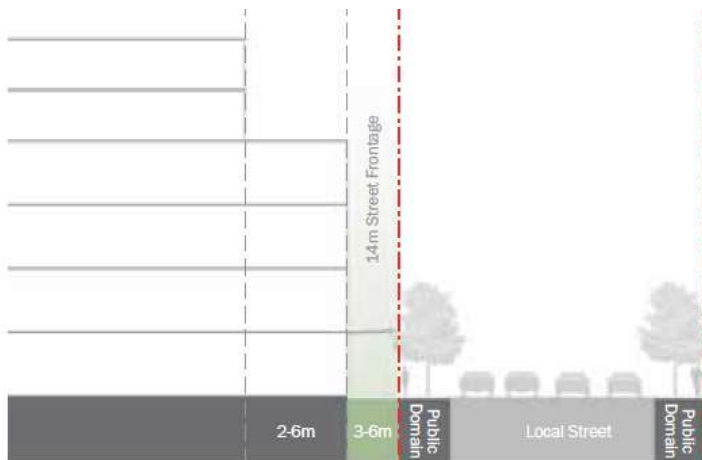
- Set back podium levels 3m from side and rear boundaries to allow for permeability
- Podium to act as visual and acoustic buffer from rail corridor and adjacent development

Tower Street Setback

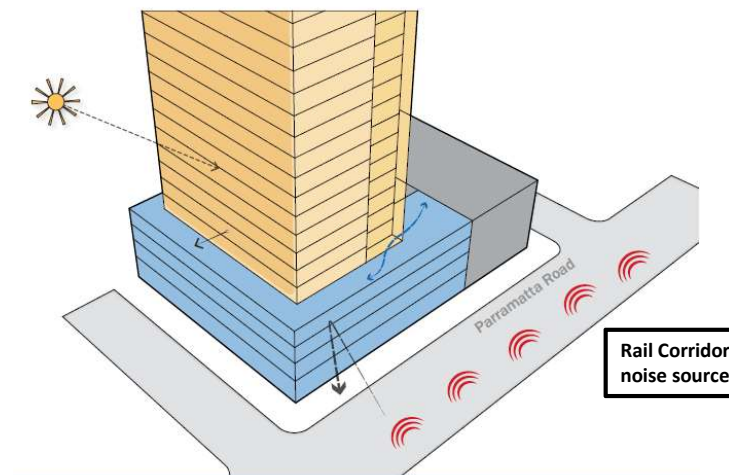
- Set back the tower footprint an additional 3m, consistent with the DCP and Strategy
- Achieves 7m overall setback from East Street boundary
- This results in lower levels proportions responding to pedestrian street scale

Tower Form

- Tower footprint less than 750m² GFA to create a slender tower form
- Tower length less than 45m to minimise overshadowing impacts
- Separation from adjacent towers should be at least 18m to allow for solar access & visual privacy
- 18m tower separation allows for quality communal open space at podium level
- Tower form should read as tall and slender above a solid base
- Tower plan should ensure visual & acoustic privacy, and comply with the requirements of the Apartment Design Guide



Podium and Tower Street Setbacks
Excerpt from Figure 4.13, PRCUTS



Building Typology - Tower above podium
Excerpt from Figure 4.21, PRCUTS

DPG6C 34-42 East St – Planning Proposal Urban Context and Design Response

4.1 Building Plan Form – Envelope Study

- 1 4m podium setback aligned with existing approved street wall position
- 2 3m setback from podium to tower
- 3 9m setback from tower to east & west boundary
- 4 3m podium setback to east and south boundaries
- 5 Slender tower envelope 17m x 35m reflects lot alignments. Maximum envelope 677m²
- 6 Podium buffer to visual and acoustic impact from rail line

Development Data

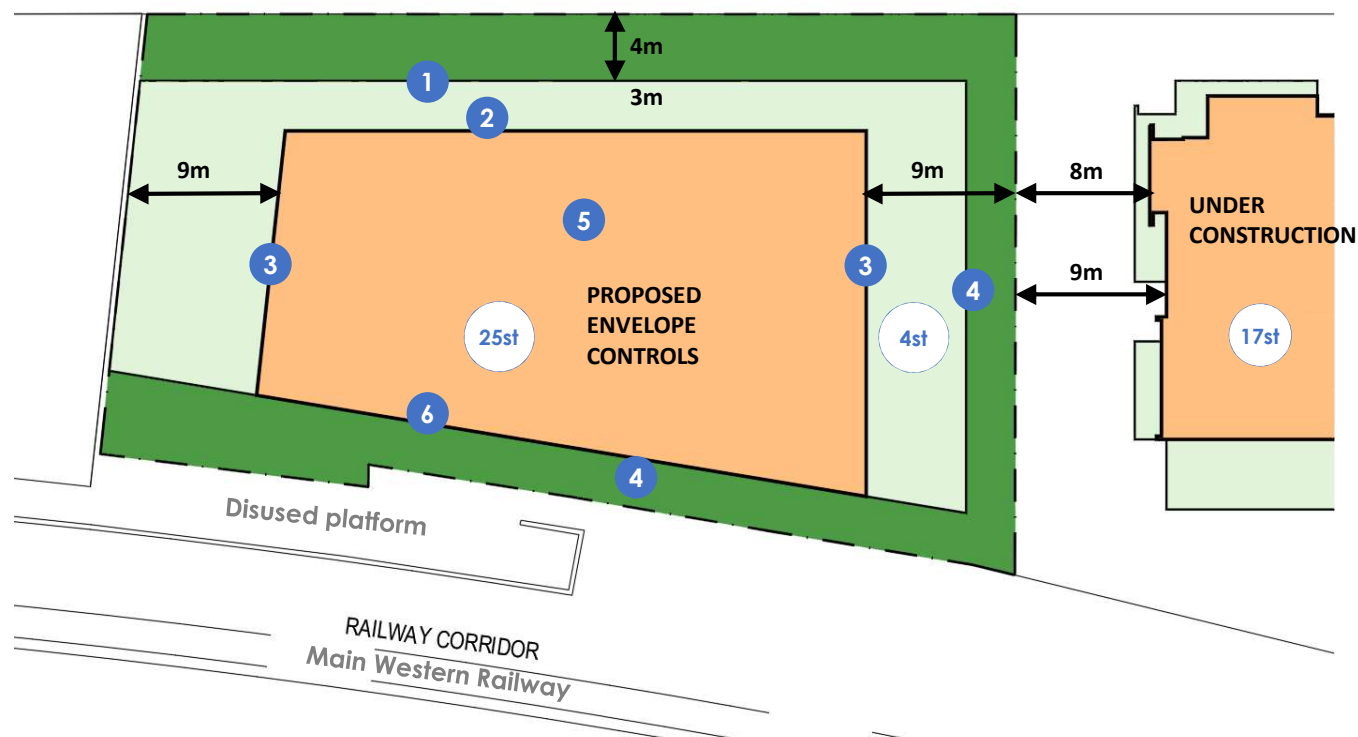
FSR	6:1
Site Area (sqm)	1,577
Target GFA (sqm)	9,460
Podium Storeys	4
Tower Storeys	22

EAST STREET
East Street

Vehicular entry



Existing Sydney
Trains Building



4.1 Building Form - Massing Study

Figure 1 – Existing approved building envelope

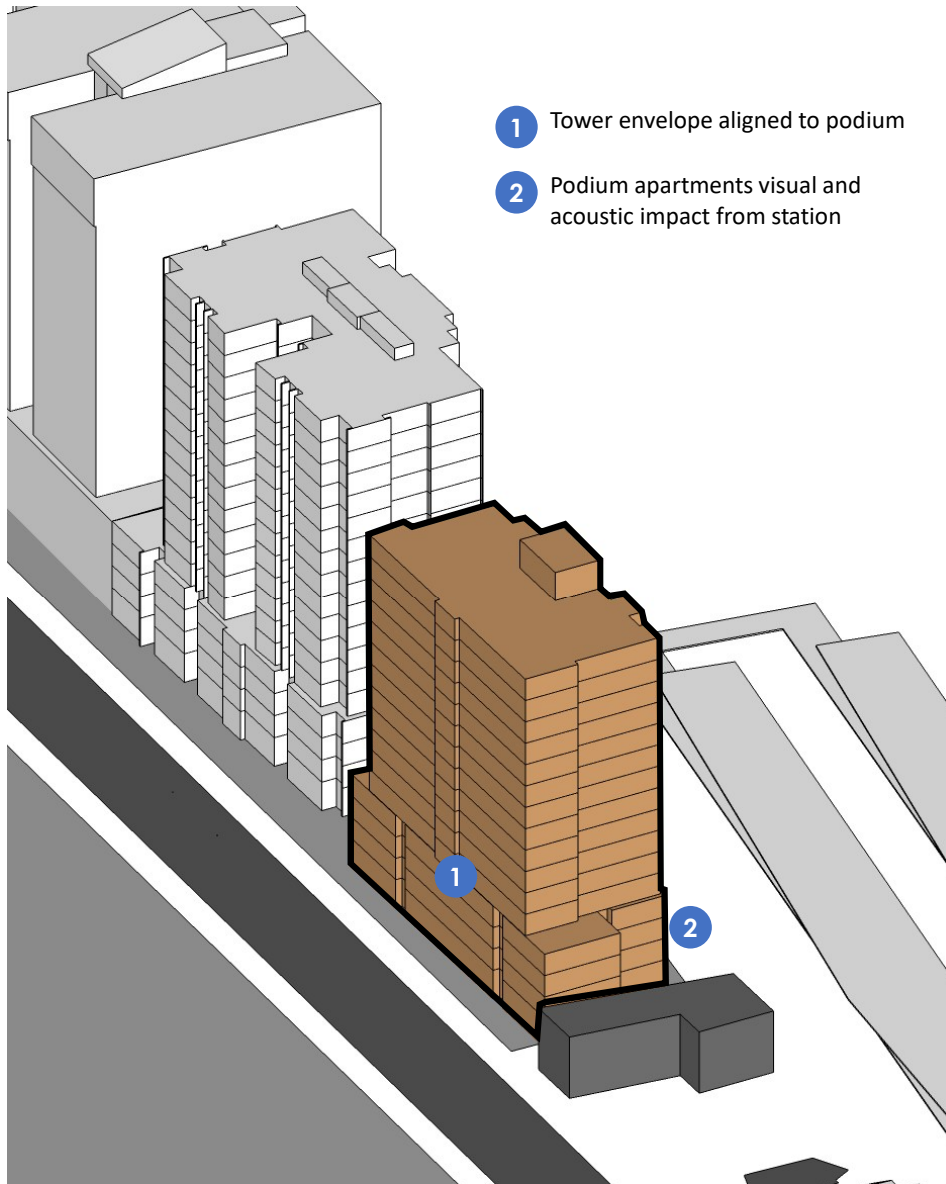
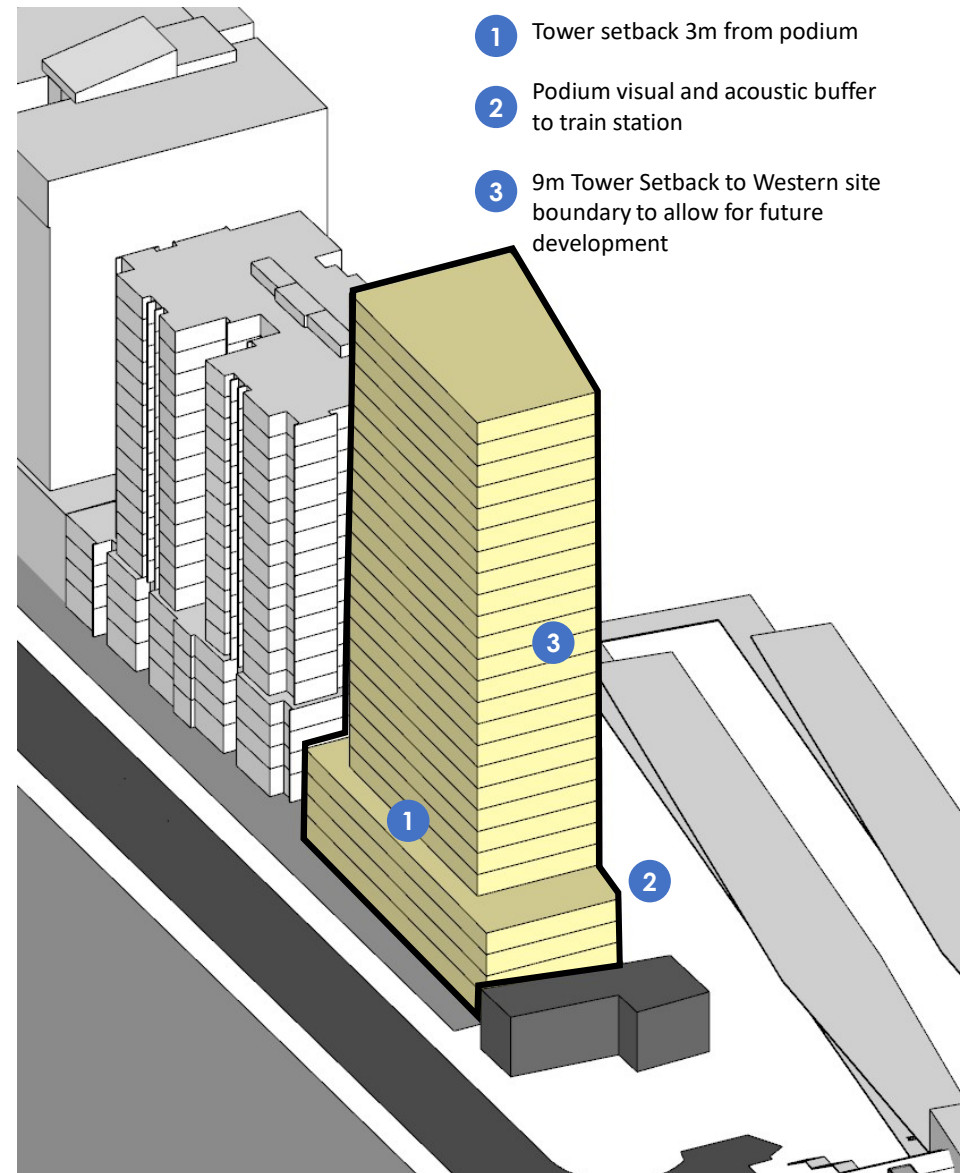


Figure 2 – Proposed additional height building envelope



4.1 Building Form - Massing Study

Figure 1 – Existing approved building envelope

- 1 Tower envelope aligned to podium
- 2 Podium apartments visual and acoustic impact from station



Figure 2 – Proposed additional height building envelope

- 1 Clear definition between tower & podium forms
- 2 Non-residential uses to podium levels to mitigate visual and acoustic privacy impacts
- 3 18m gap maintained between buildings for ADG compliance
- 4 Additional height enables a more interesting skyline through variability, and creation of a landmark identifying the train station



DPG6C 34-42 East St – Planning Proposal
Urban Context and Design Response – Shadow Studies

9am



9am 21st June Potential Shadows 52m height limit

10am



10am 21st June Potential shadows 52m height limit



9am 21st June Potential shadows 82m height limit



10am 21st June Potential shadows 82m height limit



9am 21st June Potential shadows 100m height limit



10am 21st June Potential shadows 100m height limit

DPG6C 34-42 East St – Planning Proposal
Urban Context and Design Response – Shadow Studies

11am



11am 21st June Potential Shadows 52m height limit

12am



12pm 21st June Potential shadows 52m height limit



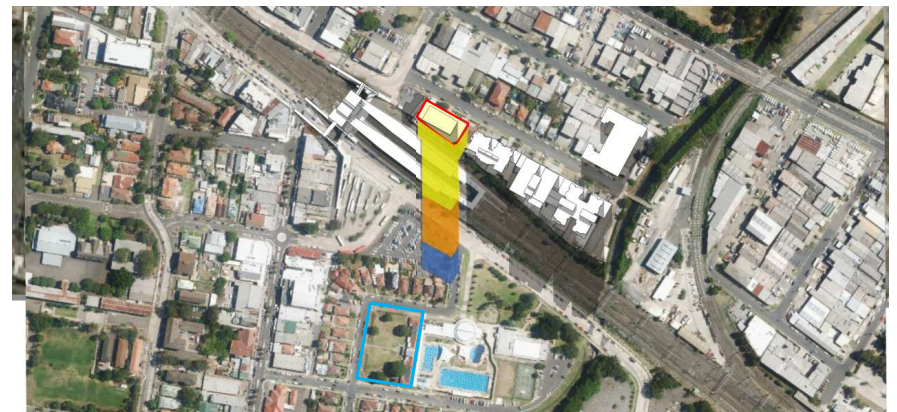
11am 21st June Potential shadows 82m height limit



12pm 21st June Potential shadows 82m height limit



11am 21st June Potential shadows 100m height limit



12pm 21st June Potential shadows 100m height limit

DPG6C 34-42 East St – Planning Proposal
Urban Context and Design Response – Shadow Studies

1pm



1pm 21st June Potential Shadows 52m height limit

2pm



2pm 21st June Potential shadows 52m height limit



1pm 21st June Potential shadows 82m height limit



2pm 21st June Potential shadows 82m height limit



1pm 21st June Potential shadows 100m height limit



2pm 21st June Potential shadows 100m height limit

3pm



3pm 21st June Potential Shadows 52m height limit



3pm 21st June Potential shadows 82m height limit



3pm 21st June Potential shadows 100m height limit

Shadow Study Conclusions

Shadows have been tested from 9am to 3pm on 21st June (Winter Solstice).

Building heights have been tested at the existing 52m height control, the proposed 82m height increase, and a potential 100m height has been included to demonstrate that a taller tower than proposed will have minimal additional overshadowing impact on uses to the south of the site.

Of particular interest is the Granville Memorial Park and swimming complex directly to the south of the subject site. The shadow study clearly shows that shadows move in an arc around the Memorial Park and pool, but do not overshadow these uses at any time during the day, even with a potential 100m tower.

The study also demonstrates that impacts from the 82m tower on residential properties are limited to a period of less than 2 hours between 9 and 11am.

The potential 100m tower has an impact across a larger area, but moves across impacted properties in roughly 1 hour.

5.0 Conclusion

This Urban context and Design Response Report demonstrates that increasing the allowable height control as recommended by *The Parramatta Road Corridor Urban Transformation Strategy* results in a better built form outcome that meets the objectives of *The Strategy* and contributes to achieving the vision for the Granville precinct.

The proposal is consistent with the existing LEP controls for FSR and land use.

The proposal provides a clear podium & tower building typology, ensuring pedestrian friendly streetscape scale, and an interesting variability to the current skyline.

The site is highly visible from the Granville Train Station and will be recognised as a landmark by thousands of train users daily.

Appendix A – Reference Design

Refer to Architectural drawings provided.

File name: 'ARCHITECTURE_34-42 East St Granville.pdf'

Appendix A – Reference Design



Figure 1 – Approved DA North Elevation

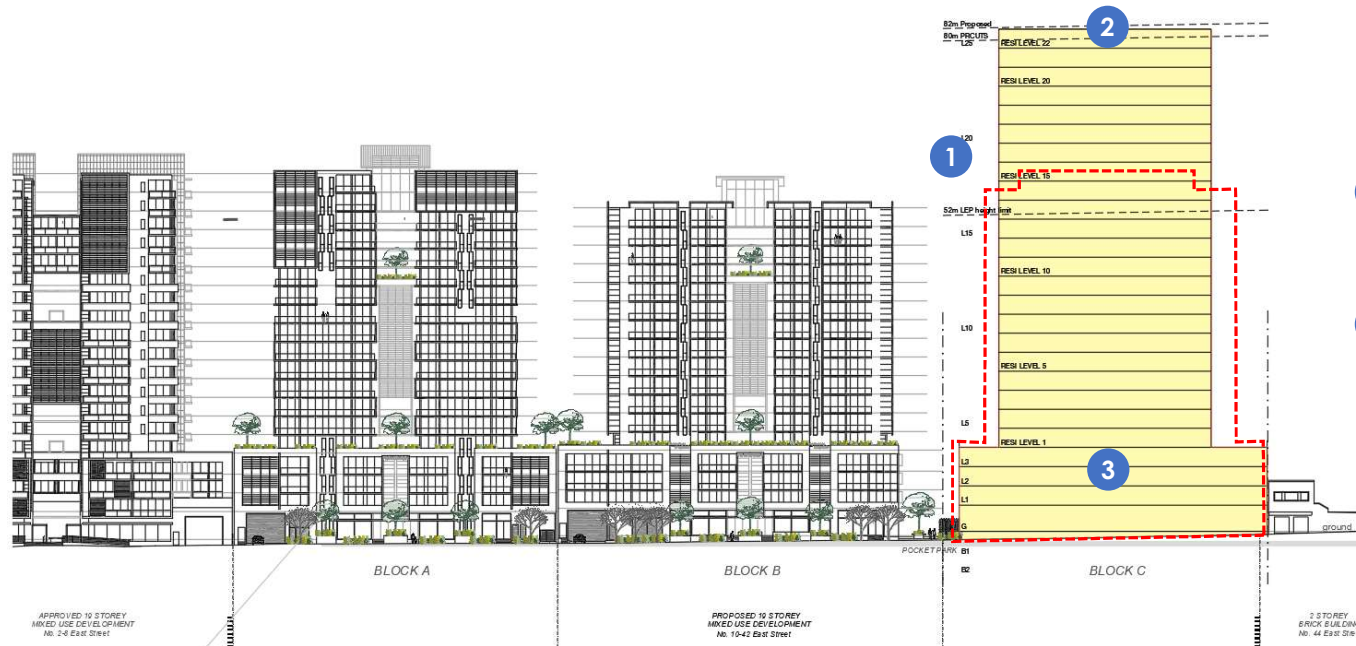


Figure 2 – Proposed additional height building envelope

- 1 18m gap between towers
- 2 Variation to skyline silhouette and creation of landmark tower adjacent rail station
- 3 Podium architecture to reflect streetscape character of surrounding approved developments – See examples on the following page

Elevation 1 : North East Street Elevation

DPG6C 34-42 East St – Planning Proposal Urban Context and Design Response

Appendix B – Podium Design

In response to the visual and acoustic impacts from the adjacent rail corridor, and taking advantage of the additional height, car parking has been designed in the upper two podium levels.

There are a number of benefits and opportunities to this arrangement:

- Eliminates overlooking and privacy issues between train commuters on platforms at Granville Station
- Provides an acoustic buffer to rail noise impacts
- Allows for natural ventilation to parking levels reducing energy use
- Excavation deeper than 2 levels is not practical on this site due to inability to install temporary rock anchors on three sides of the basement.

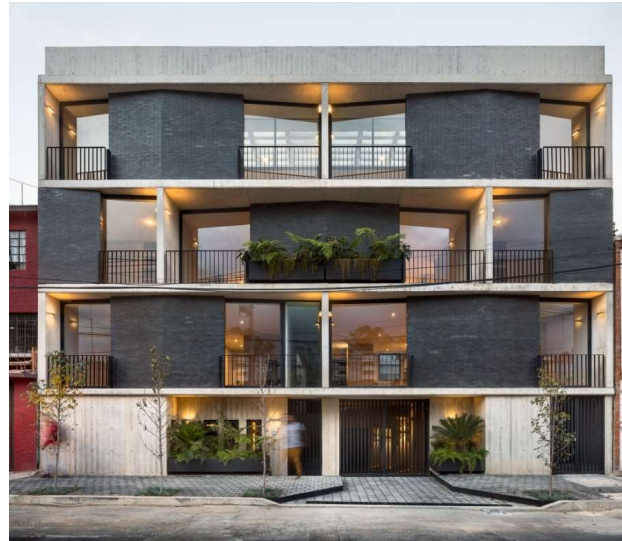
There is a risk with podium parking that inadequate or poorly detailed screening will lead to a poor interface for pedestrians and streetscape.

Suggestions to avoid this outcome include:

- Include modular proportions reflecting the predominant building fine grain in the precinct, particularly along Good St
- Create walls and openings to give the appearance of commercial or residential uses
- Specifying quality finishes
- Providing safe, easy access for maintenance
- Incorporating public art
- Incorporating landscape planting elements to encourage seasonal variation
- Avoid direct line of site from the street into the carpark
- Continue the Level 1 façade up to balustrade height of level 2 to partially conceal it

Some potential façade treatments are illustrated in the accompanying images.

A detailed façade study will be provided with a Development Application.



Break up façade into smaller modules



Break up façade into smaller modules



Example elevation from approved
development on north side of East St

DPG6C 34-42 East St – Planning Proposal Urban Context and Design Response

Appendix C – GFA & Parking Calculations

Level	Height	GBA	GFA	NSA	Units	
		Roof Plantroom				
25	3.05	Resi 22	260	195	176	2
24	3.05	Resi 21	550	413	371	5
23	3.05	Resi 20	550	413	371	5
22	3.05	Resi 19	550	413	371	5
21	3.05	Resi 18	550	413	371	5
20	3.05	Resi 17	550	413	371	5
19	3.05	Resi 16	550	413	371	5
18	3.05	Resi 15	550	413	371	5
17	3.05	Resi 14	550	413	371	5
16	3.05	Resi 13	550	413	371	5
15	3.05	Resi 12	550	413	371	6
14	3.05	Resi 11	550	413	371	6
13	3.05	Resi 10	550	413	371	6
12	3.05	Resi 9	550	413	371	6
11	3.05	Resi 8	550	413	371	6
10	3.05	Resi 7	550	413	371	6
9	3.05	Resi 6	550	413	371	6
8	3.05	Resi 5	550	413	371	6
7	3.05	Resi 4	550	413	371	6
6	3.05	Resi 3	550	413	371	6
5	3.05	Resi 2	550	413	371	6
4	3.05	Resi 1	550	413	371	6
3	3.05	P3	1091	0	0	27
2	3.05	P2	1091	0	0	23
1	4	P1	1091	350	0	
G	4.2	G - Retail	940	250	295	
B1		B1	1091	0	0	23
B2		B2	1091	0	0	28
101 cars						
Total	81.4m max 82m	15,083	9,458 max 9460	8,267	119	

Parking Generation Rates		
	PRC max	DCP min
Studio	0.3	0
1 bed	0.5	1
2 bed	0.9	1
3 bed	1.2	1.2
Visitor	0.1	0.25
Commercial	100	50
Retail	70	60

Parking	# Units	PRC	DCP	Actual
Studio	24	7	0	0
1 bed	21	11	21	19
2 bed	63	57	63	63
3 bed	11	13	13	11
Motorcycle	5			
Visitor	119	11.9	30	12
Commercial	350	4	7	4
Retail	250	4	4.2	4
Total Parking	max	107	138	101

Residential parking is provided roughly in accordance with the PRCUTS rates

Commercial parking is provided in accordance with the PRCUTS generation rates. Overall provision falls just below the PRCUTS maximum rate.

The DA approval included 123 parking spaces.

This proposal reduces the overall parking provision, encouraging alternative forms of transport.

R O W E L L

21-41 EAST ST
APPROVED DEVELOPMENT
CONSTRUCTION STARTING 2017 / 2018

PUBLIC DOMAIN
- New public domain finishes to match adjacent developments to create a holistic streetscape

POCKET PARK
- Overall dimensions approximately 12m wide frontage x 14.6m depth
- Existing substation can be screened with decorative fencing similar to the approved DA and still allow circulation and visual connection between new retail tenancies and the pocket park.
- New corner retail tenancy interacts with both East St and the pocket park

10-32 EAST ST
APPROVED DEVELOPMENT
UNDER CONSTRUCTION

develotek
PROPERTY GROUP

PROJECT NAME

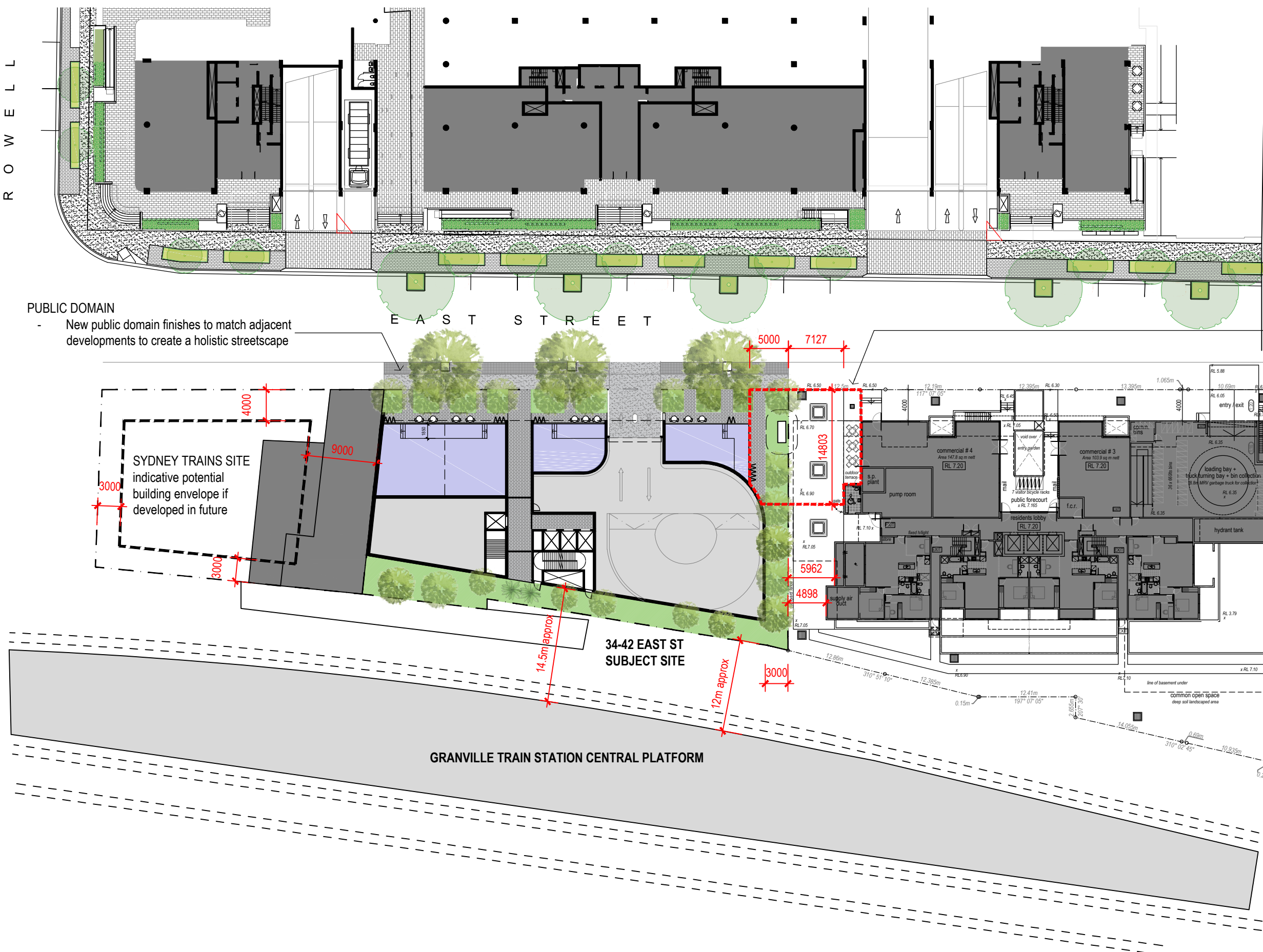
**DPG 6 34 - 42
EAST ST, GRANVILLE**

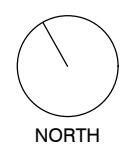
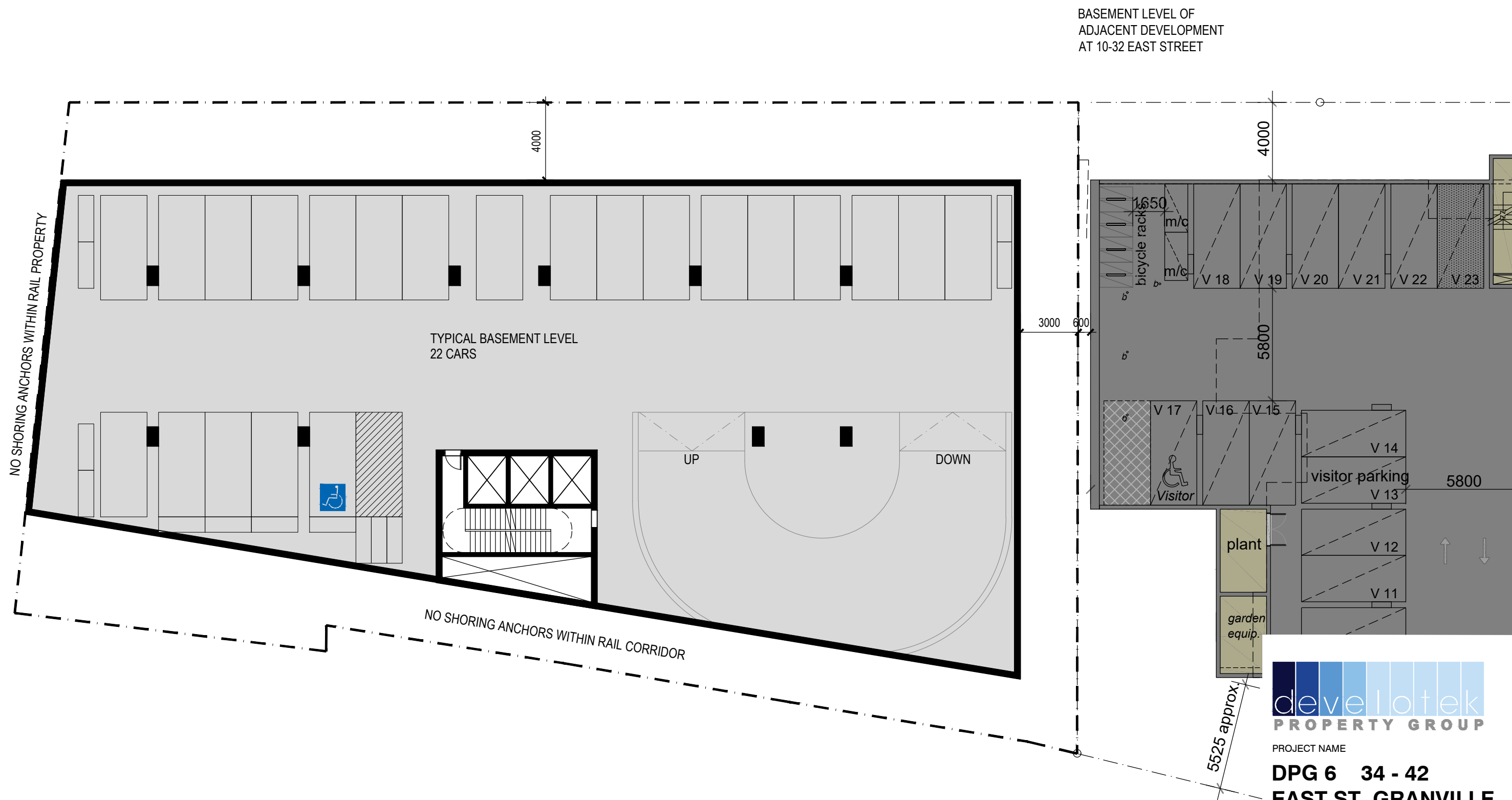
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CONTEXT PLAN

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DWG NUMBER
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REV
04





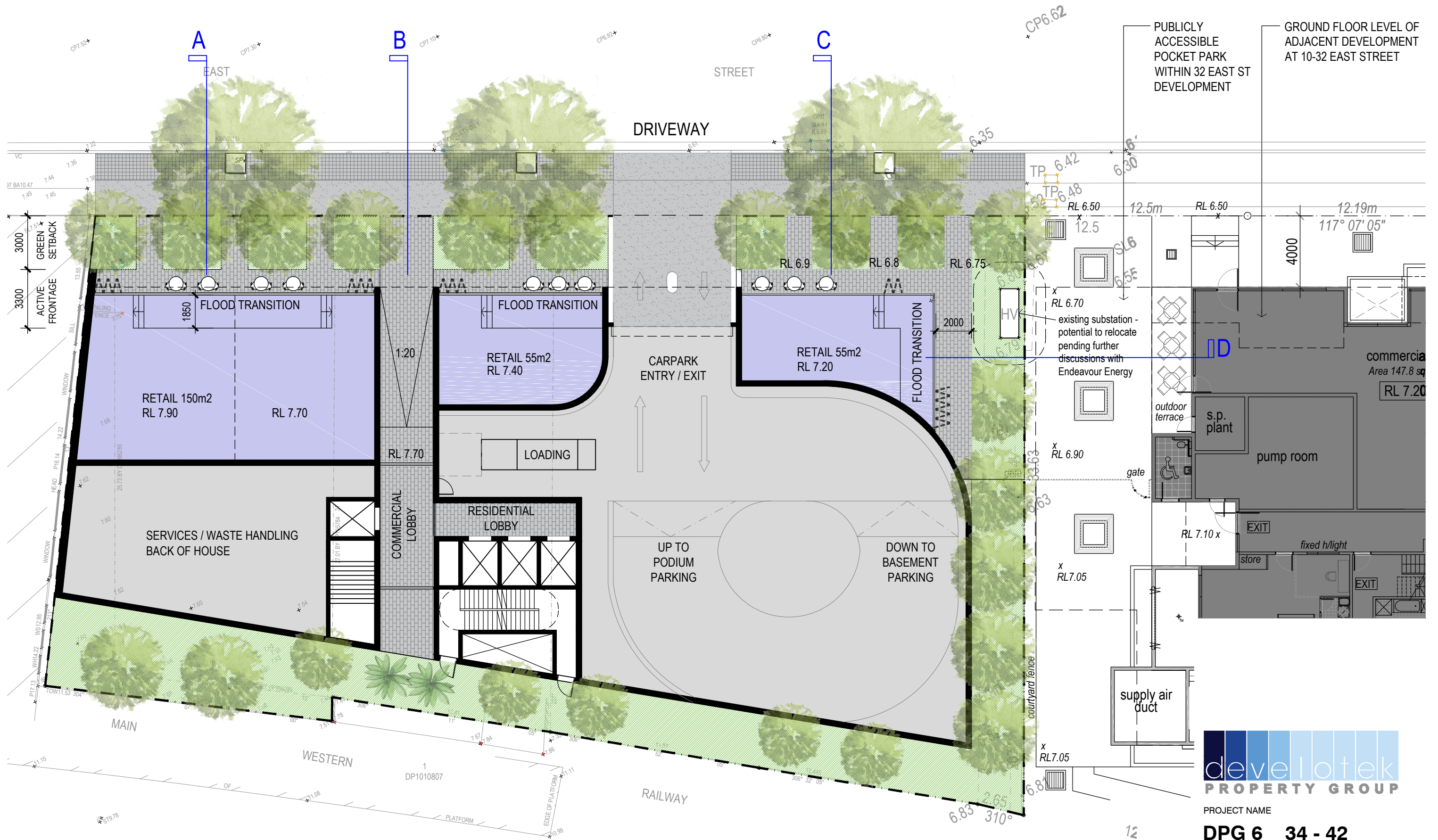
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EAST ST, GRANVILLE

DRAWING TITLE
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PP01

REV
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PROJECT NAME

**DPG 6 34 - 42
EAST ST, GRANVILLE**

DRAWING TITLE

GROUND PLAN

DATE

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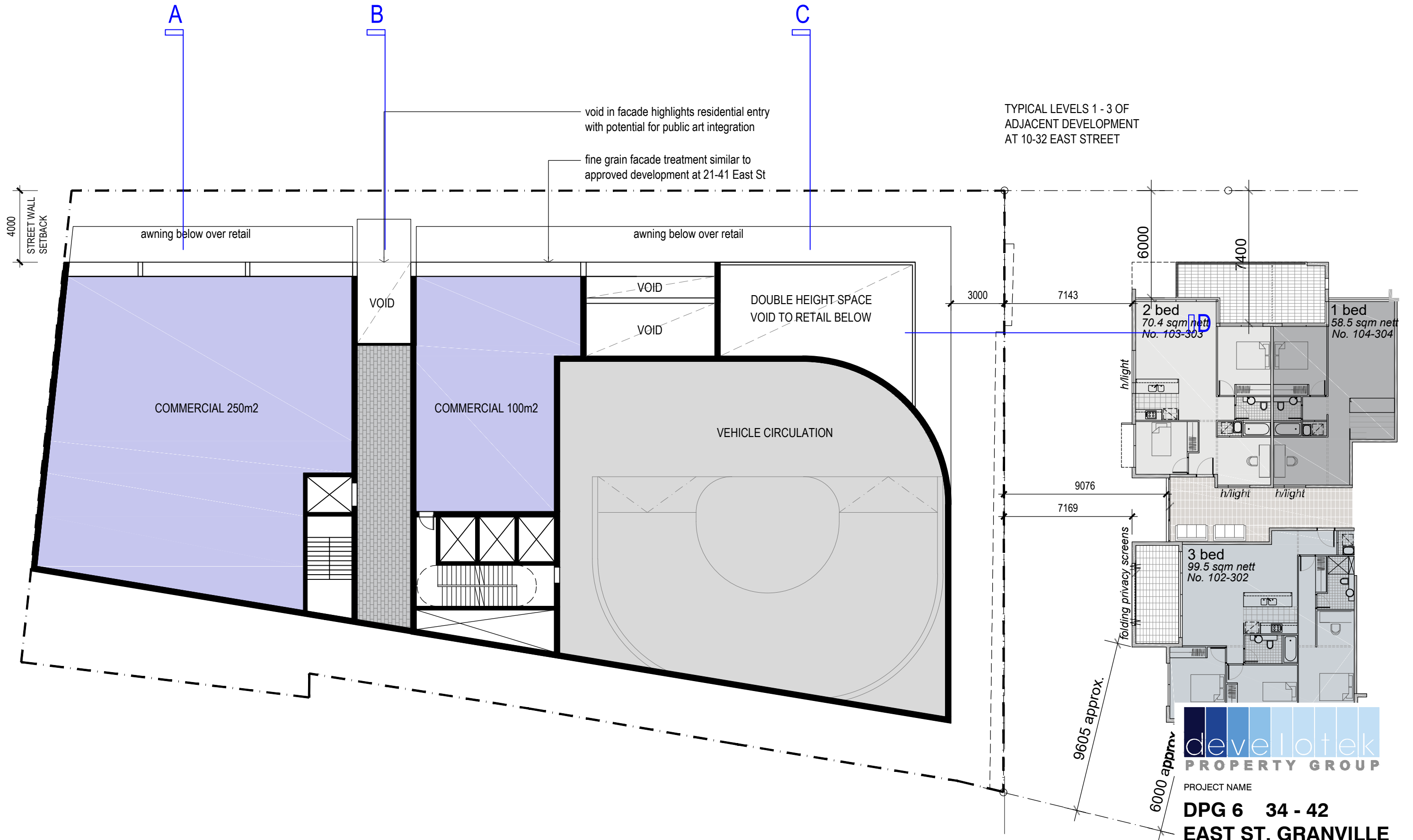
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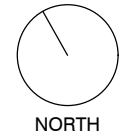
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PROJECT NAME
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EAST ST, GRANVILLE

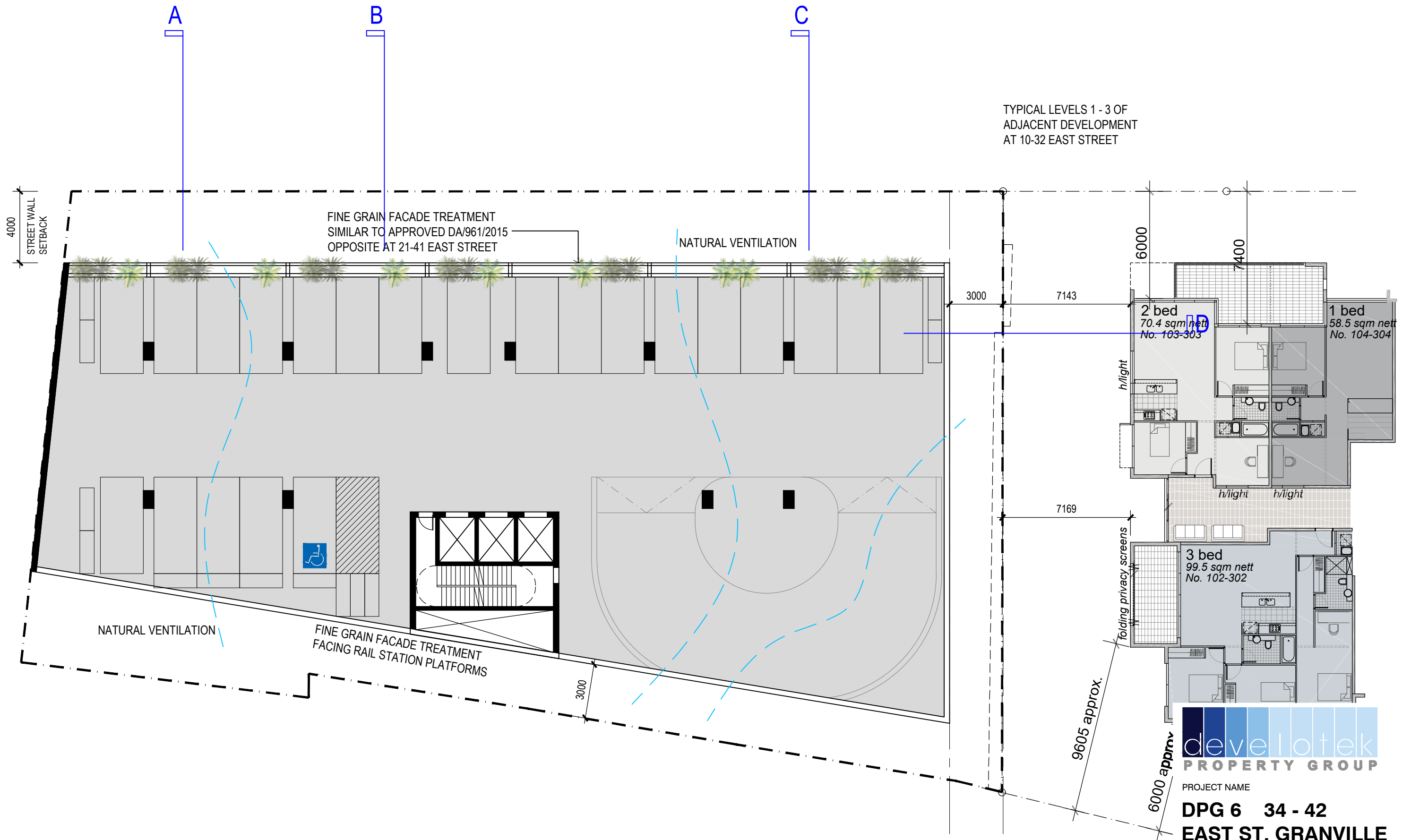
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DATE
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SCALE
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DWG NUMBER
PP03

REV
04



TYPICAL LEVELS 1 - 3 OF
ADJACENT DEVELOPMENT
AT 10-32 EAST STREET



PROJECT NAME

DPG 6 34 - 42
EAST ST, GRANVILLE

DRAWING TITLE

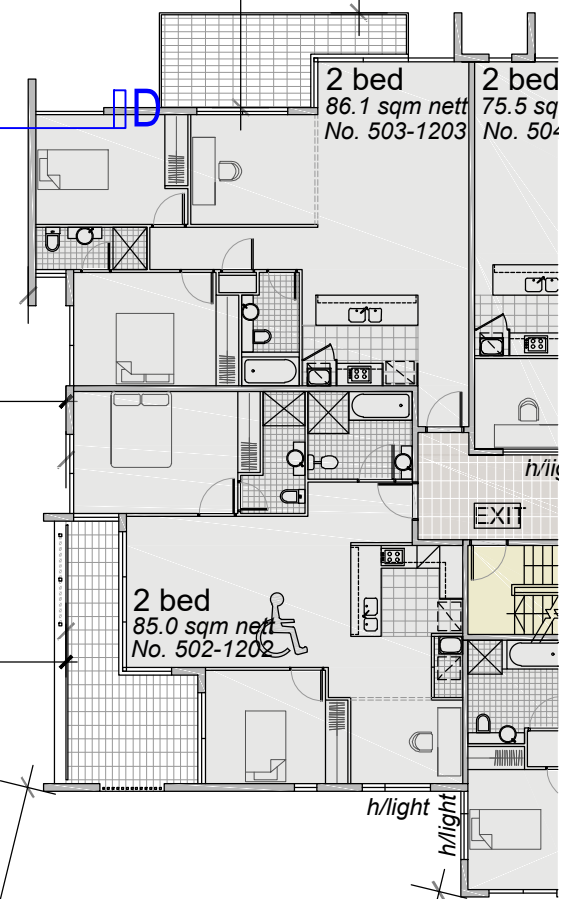
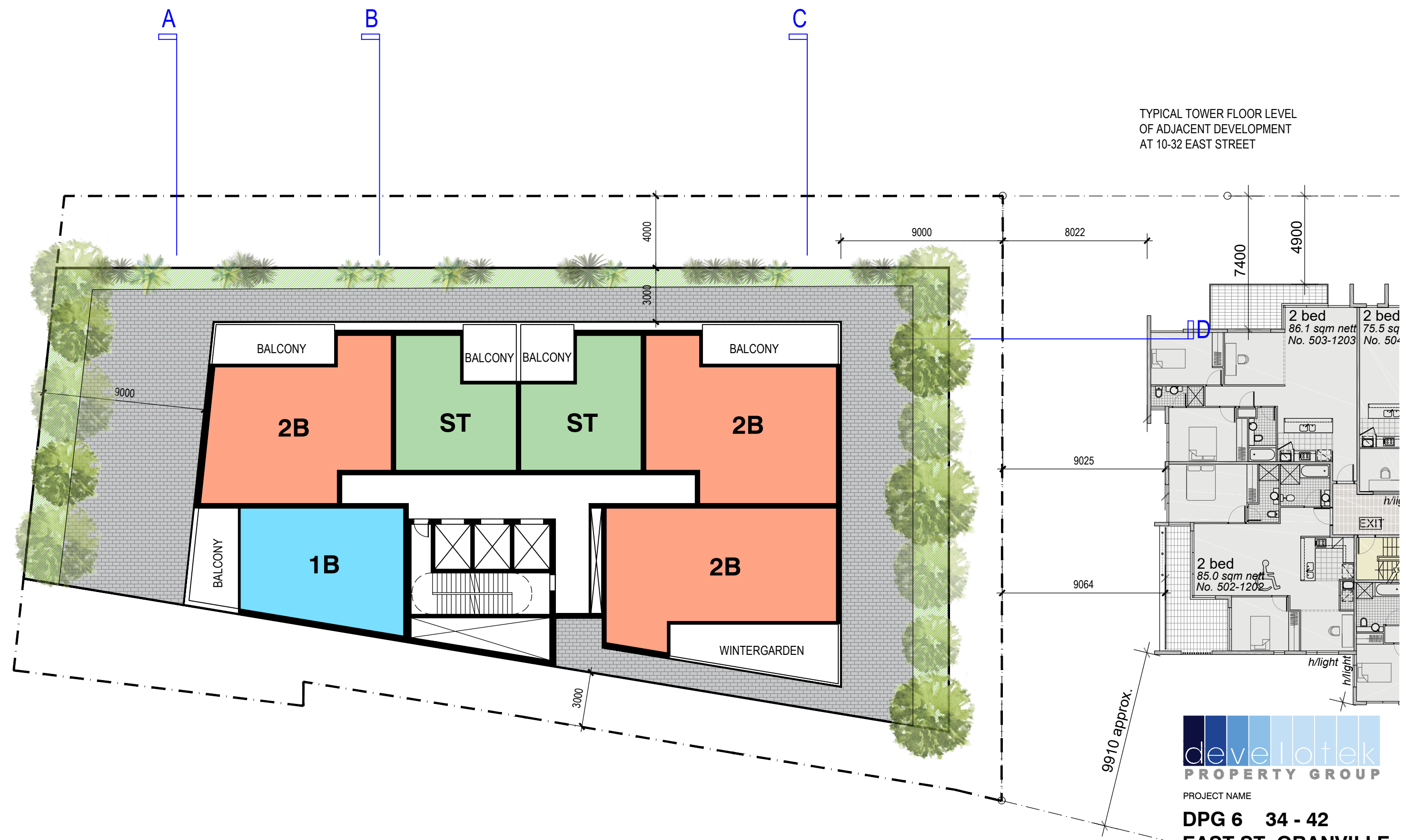
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DATE
30.08.17
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DWG NUMBER
PP04
REV
04

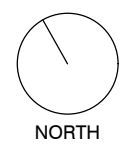


TYPICAL TOWER FLOOR LEVEL
OF ADJACENT DEVELOPMENT
AT 10-32 EAST STREET



PROJECT NAME
DPG 6 34 - 42
EAST ST, GRANVILLE

DRAWING TITLE
L4- L16 PLAN



DATE	DWG NUMBER	REV
30.08.17	PP05	04
SCALE		
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ROOF LEVEL OF
ADJACENT DEVELOPMENT
AT 10-32 EAST STREET

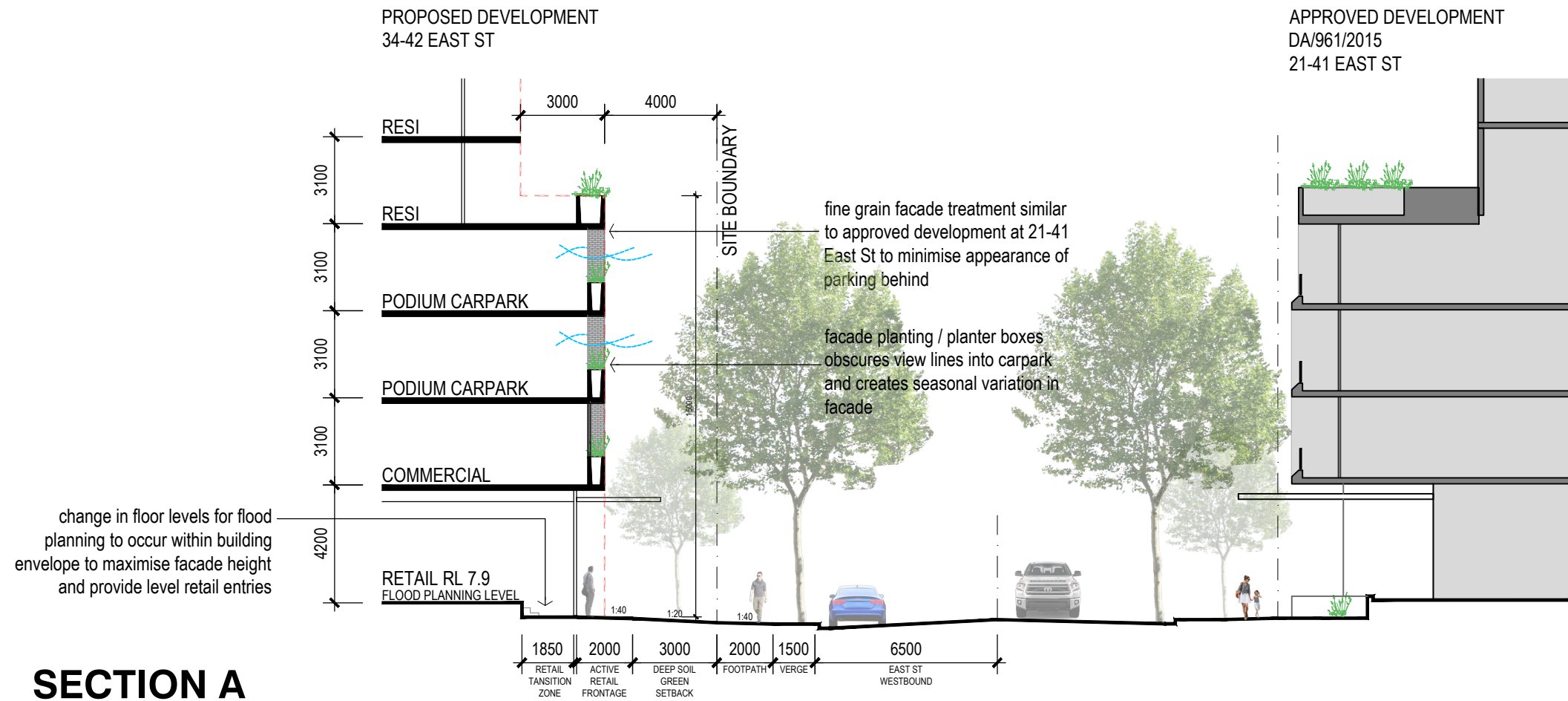


PROJECT NAME
DPG 6 34 - 42
EAST ST, GRANVILLE

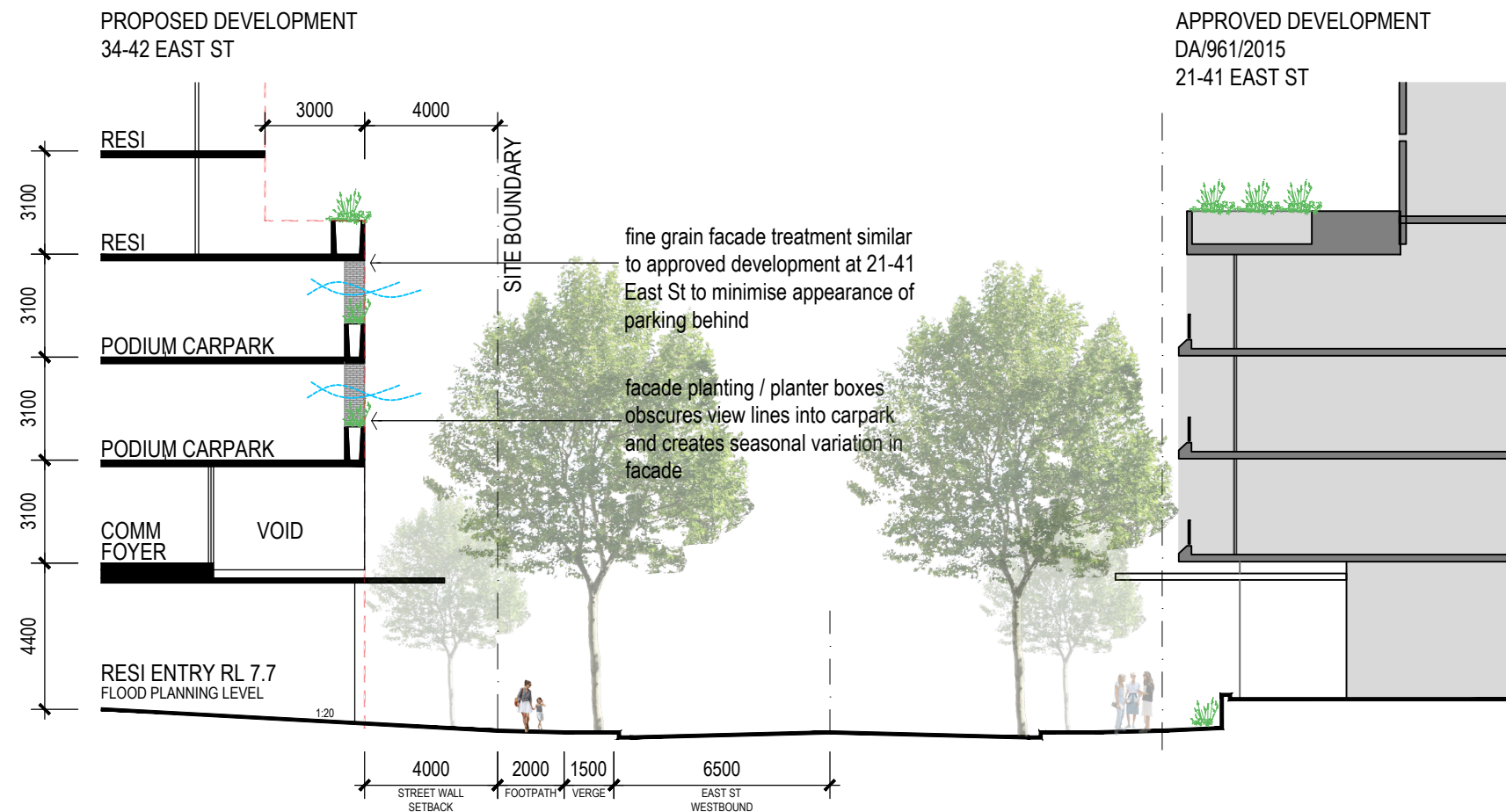
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L17-L25 PLAN



DATE	DWG NUMBER	REV
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SECTION A



SECTION B



PROJECT NAME

**DPG 6 34 - 42
EAST ST, GRANVILLE**

DRAWING TITLE

STREET SECTIONS

DATE

30.08.17

SCALE

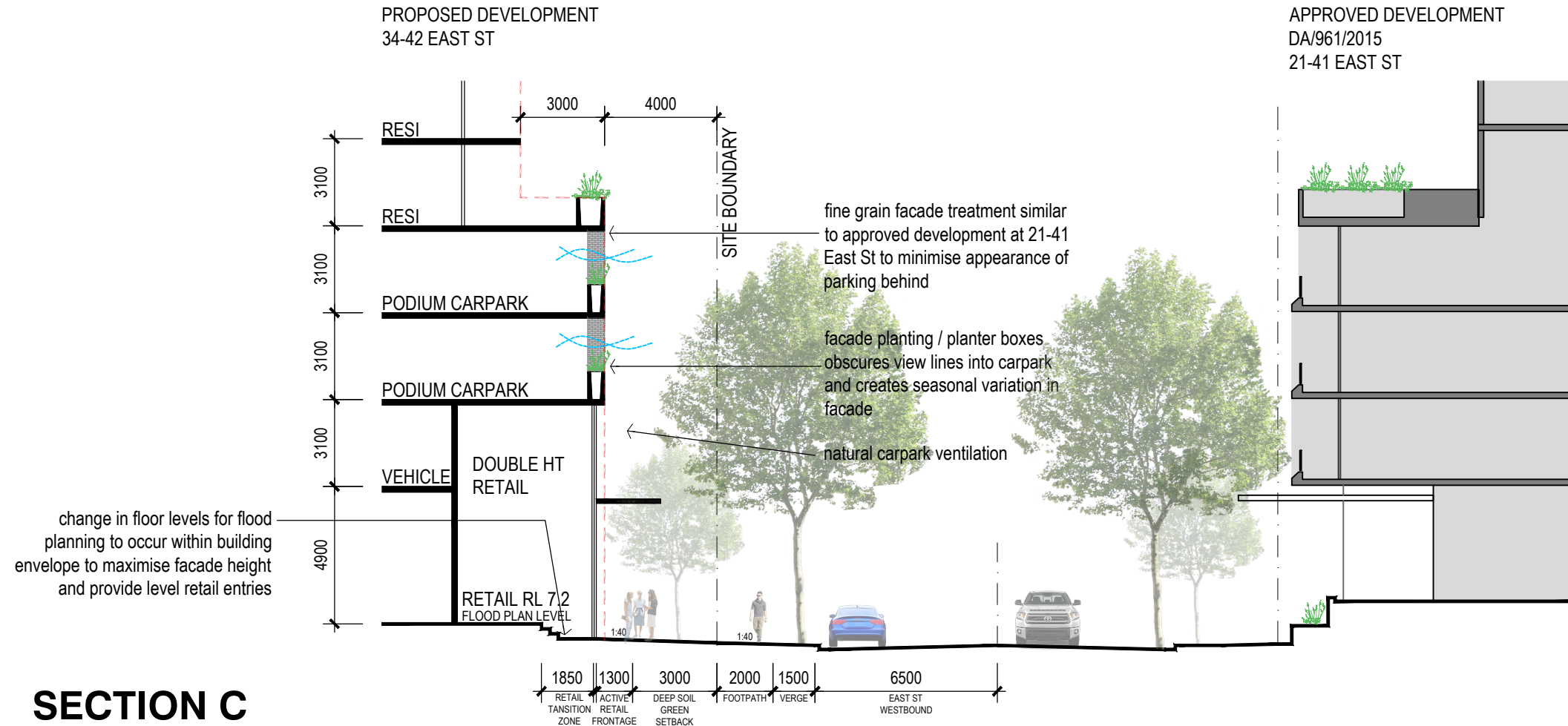
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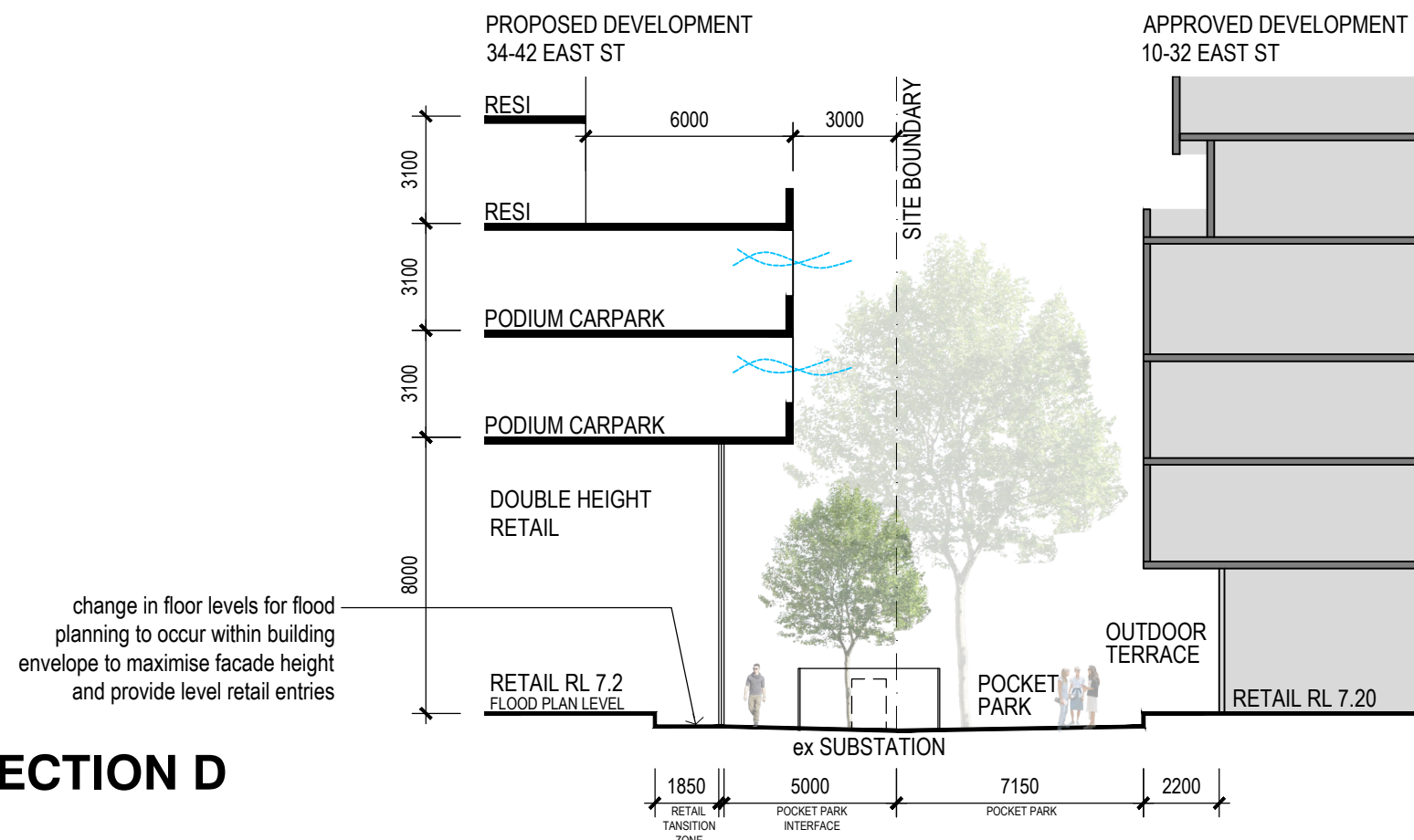
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SECTION C



SECTION D



PROJECT NAME

DPG 6 34 - 42
EAST ST, GRANVILLE

DRAWING TITLE

STREET SECTIONS

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30.08.17
SCALE
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DWG NUMBER
PP08
REV
04

Appendix 2 – Preliminary Site Investigation

Appendix 2 has not been included in the Business Paper due to the size. For access to this document please contact Council's Project Officer, Terry Agar at tagar@parracity.nsw.gov.au

Appendix 3 – Flood Study

Appendix 3 has not been included in the Business Paper due to the size. For access to this document please contact Council's Project Officer, Terry Agar at tagar@parracity.nsw.gov.au

Appendix 4 – Traffic Statement

Appendix 4 has not been included in the Business Paper due to the size. For access to this document please contact Council's Project Officer, Terry Agar at tagar@parracity.nsw.gov.au

Appendix 5 – Heritage Statement

Appendix 5 has not been included in the Business Paper due to the size. For access to this document please contact Council's Project Officer, Terry Agar at tagar@parracity.nsw.gov.au



Prepared by City of Parramatta

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